

City of Deltona

2345 Providence Blvd.
Deltona, FL 32725

Agenda

Ordinance Review Committee

Chair Melissa Roberts
Vice Chair Sonjia Kihlmire
Member Joseph Cerrato
Member James Diehl
Member Cheri Taylor
Member Thomas Walsh
Member Rossana Sepulveda

Thursday, April 7, 2016

6:00 PM

Second Floor Conference Room

1. CALL TO ORDER:

2. ROLL CALL

3. APPROVAL OF MINUTES & AGENDA:

A. [Minutes for March 3, 2016.](#)

4. PRESENTATIONS/AWARDS/REPORTS:

5. PUBLIC FORUM:

6. OLD BUSINESS:

A. [Sec. 110-828. - Off-street parking and loading. \(Regulations\)](#)

7. NEW BUSINESS:

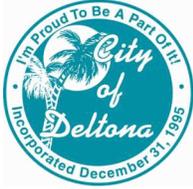
8. STAFF COMMENTS:

9. BOARD/COMMITTEE MEMBERS COMMENTS:

10. ADJOURNMENT:

NOTE: If any person decides to appeal any decision made by the City Commission with respect to any matter considered at this meeting or hearing, he/she will need a record of the proceedings, and for such purpose he/she may need to ensure that a verbatim record of the proceedings is made, which record includes the testimony and evidence upon which the appeal is to be based (F.S. 286.0105).

Individuals with disabilities needing assistance to participate in any of these proceedings should contact the City Clerk, Joyce Raftery 48 hours in advance of the meeting date and time at (386) 878-8500.



City of Deltona

2345 Providence Blvd.
Deltona, FL 32725

DRAFT Minutes - Draft Ordinance Review Committee

Thursday, March 3, 2016

6:00 PM

Second Floor Conference Room

1. CALL TO ORDER:

The meeting was called to order at 6:00 pm.

2. ROLL CALL:

Also present: Chris Bowley, Planning and Development Services and Kathrine Kyp, Planning and Development Services.

Present: 6 - Vice Chair Sonjia Kihlmire
Member Joseph Cerrato
Member Cheri Taylor
Member Thomas Walsh
Chair Melissa Roberts
Member Rossana Sepulveda

3. APPROVAL OF MINUTES & AGENDA:

A. Minutes for January 7, 2016

Motion by Member Cerrato, seconded by Member Taylor, to approve the minutes for January 7, 2016. The motion carried by the following vote:

For: 6 - Vice Chair Kihlmire, Member Cerrato, Member Taylor, Member Walsh, Chair Roberts and Member Sepulveda

4. PRESENTATIONS/AWARDS/REPORTS:

5. PUBLIC FORUM:

6. OLD BUSINESS:

7. NEW BUSINESS:

A. Election of Officers

Member Cerrato nominated Member Roberts for the position of Chairman with Member Taylor seconded. With no further nominations, Member Roberts accepted the nomination of Chairman.

B. Discussion: Sec. 110-828. - Off-street parking and loading. (Regulations)

Mr. Bowley provided the Board with an update of Ordinance 09-2015 and the dates of the public hearings with the Planning and Zoning Board and City Commission. Discussion ensued regarding the powers and duties of the Planning and Zoning Board and the process if the Board makes changes. Mr. Bowley noted, that all recommendations and changes will be forwarded on to the City Commission to make the final determination.

Mr. Bowley provided the Board with a summary of how Deltona was platted and the difficulties of converting residential lots to retrofitted non-residentially used lots. He stated that, the committee's main objective with this section of the code is only focused on vehicle parking on residential lots. Discussion by the Board regarding the difficulties of retro-fitting today's lifestyle onto older lots ensued.

Discussion amongst the Board and Mr. Bowley occurred with Mr. Bowley asking the Board to focus on the allowance of a parking pad, the quantity of parking to allow, prohibiting parking on the side of the house, properly accessing the property, mindful of utility locations, the slope of the lots and the importance of a stabilized surface for parking. Mr. Bowley suggested that the Board continue to review this section of the code with these elements in mind and bring it back again next month prior to drafting an ordinance. He asked that the board submit changes or additions to the code to Ms. Kyp to compile for next meeting.

8. STAFF COMMENTS:**9. BOARD/COMMITTEE MEMBERS COMMENTS:****10. ADJOURNMENT:**

The meeting was adjourned at 7:17 pm.

Melissa Roberts, Chairperson

ATTEST:

Kathrine Kyp, Board Secretary

Sec. 110-828. - Off-street parking and loading. (Regulations)

- (a) Off-street parking and loading regulations. Where required by this chapter, every use or structure shall have an adequate number of off-street parking and loading spaces for the use of occupants, employees, visitors, customers, patrons or suppliers. Except as noted in this section, chapter 96, article II, Code of Ordinances shall apply to the design and construction of all required off-street parking and loading areas. .
- (b) Surfacing, drainage, lighting, and access.
- (1) For single-family and two-family (duplex) residential dwellings, for model homes and model home parking lots located in residential zoning categories, and for city-owned facilities and Volusia County, or State of Florida, park and recreation facilities, offstreet parking areas and driveways that connect to paved streets or roads shall be surfaced with either concrete, brick, or asphalt, and maintained in a good condition for a minimum distance of 35 feet, or the distance to the house from the front or street side lot line, whichever is less. Additional driveways, driveways that connect to unpaved streets or roads, and parking spaces on these sites may be surfaced with crushed rock, shell, or stone, and maintained in a smooth well-graded condition.

Melissa suggestion add mulch, brick pavers

Material used at grades exceeding five percent (20'h.:1'v) must be attached to the ground, and may include paving brick or stone. Borders shall include provisions to ensure stormwater runoff is allowed to flow into the yard area adjacent to the driveway. Stormwater shall not be directed down the driveway into the public or private right-of-way or access easement except to the minimum extent necessary to effect a connection to the driveway apron at the property line, or onto adjacent property. The design storm event shall be as specified in the Deltona Land Development Code as it may be amended from time to time.

Joe suggestion Based on the minimum/maximum spaces – would a standard driveway of a single family home that can accommodate 4 vehicles be in violation of the maximum allowable off-street parking spaces (see Table 110-9).

Tom's Jan 27, comment: "Materials used at grades exceeding five percent, etc." While I understand and agree with the intention of the driveway border regulation, I question whether it is practical if the driveway is flat as virtually all driveways are. In short, it seems this paragraph will do little more than to make it unlawful for water to run downhill.

Joe suggestion b)(1) – add sentence as follows: Any additional parking adjacent to a paved or concrete driveway may be surfaced with mulch, rock, shell or stone and maintained in a smooth well-graded condition.

- (2) Any required off-street parking and loading areas, including overflow areas, for land uses other than single-family or two-family (duplex) dwellings shall be surfaced with brick, asphalt, bituminous, or concrete material and maintained in a smooth condition.

Melissa suggestion add mulch,rock – & Question what about Church facilities that have dirt/pine needle and wood markers for parking spaces?

All areas shall be designed for the safety and convenient access of pedestrians and vehicles. An illumination plan prepared by a licensed professional engineer with expertise in the field of illumination, including the latest illumination technology available, shall be submitted for each parking facility that is proposed to be illuminated. Lights, used to illuminate any off-street parking facility, shall be designed and installed to prevent a related hazard or nuisance to vehicular or air traffic and to prevent glare, annoyance or discomfort by directing light away from

adjacent residential properties and adjoining streets. In no case shall illumination from a parking facility (including illuminated canopies) increase the level of illumination at the edge of pavement on adjacent streets or at the property lines of adjoining properties by more than one foot candle. The maximum height of the light fixture, including pole and lamphhead, shall not exceed 35 feet and be erected such that any series of light poles and lamphheads are equal in height, as measured from ground level.

- (3) To promote the safety of vehicular traffic and pedestrians and to minimize traffic congestion and conflict, access to any project or development, including single-family homes and duplexes, shall comply with the requirements of the Land Development Code, chapter 96, article II, as it may be amended from time to time, and shall require a hard surface driveway apron meeting city engineering standards connecting the driveway to a public street.

(c) Location.

- (1) The minimum number of parking spaces required in section 110-828(f) for all single-family and two-family dwellings shall be located on the same lot as the main building. If additional parking spaces are required for any single-family or two-family dwelling, the additional parking spaces may be located either on the same lot as the main building, or on an adjacent vacant lot of an expanded residential building site. If the required off-street parking spaces for all other uses cannot reasonably be provided on the same lot on which the principal building or use is located, such required off-street parking spaces may be located on another lot, owned or leased by the owner of the lot on which the principal structure or use is located, provided that such spaces are located within 200 feet of the premises to be served, and, are located only in one or more of the following classifications: RM-1, RM-2, OR, C-1, C-2, C-3, I, PUD or PB. Such spaces may be located in any single-family residential zoning district only as a conditional use. Heavy equipment and vehicles requiring a commercial drivers' license of any class shall not be parked or stored on an off-premises parking lot permitted by this paragraph within any residential zoning district, or within the professional business zoning classification.
- (2) No parking space or portion of any parking facility shall be located or built within any platted easement unless an authorized use permit is issued by the City of Deltona.

(d) Plan requirement. An off-street parking or loading space plan shall be submitted as follows:

- (1) For single-family and duplex uses off-street parking plans shall be shown on the plot plans submitted with an application for a building or zoning permit. The plot plan shall accurately illustrate the number and location of parking spaces and driveways. The addition of parking spaces to an existing single-family or duplex residential building site shall require an application for a zoning permit, which shall include a plot plan and sealed survey of the expanded residential building site. All required landscaping and screening, areas proposed to be cleared, trees proposed to be removed, existing and proposed vehicle accessways, parking areas, and structures shall be shown on the plot plan. In addition, building permit applications shall be submitted for any proposed construction that is regulated by the city's adopted building code.
- (2) For all other uses, an off-street parking and loading space plan meeting the requirements of chapter 96, article II, Code of Ordinances shall be submitted and approved during the site plan review process of the Land Development Code.

(e) Design requirements for off-street parking areas. Off-street parking areas shall be designed and located to meet the following requirements:

- (1) For single-family and duplex uses, except as otherwise provided in this article for expanded residential building sites, each off-street parking space shall be located on the premises which it serves; have minimum dimensions of nine feet in width by 19 feet in depth; not be located in any front yard except on a driveway but may be located within any garage or carport on the premises; and/or, may be located within any side or rear yard but not closer than five feet to any side or rear lot line, but not in any platted easements unless an authorized use permit is issued by the City of Deltona. ~~Each such space must be accessible from a driveway connected to the~~

~~street providing primary access to the premises.~~ Melissa suggest to omit due to the unique property designs of homes in Deltona

The design requirements for parking on the vacant lot of an expanded residential building site are set forth in paragraph (2) of this section.

- (2) When additional parking is installed on the vacant lot of an expanded residential building site, the parking area shall be designed and built in accordance with the requirements of this paragraph, as follows:
 - a. Surfacing, drainage and access. Surfacing, drainage and access for any parking area on the adjacent vacant lot of an expanded residential building site shall meet the requirements of subsection 110-828(b).
 - b. Driveway spacing. Driveway spacing shall meet the minimum standards of the Deltona Land Development Code, Ordinance 96-25, as it may be amended from time to time. No driveway connection to a street may be made to the vacant portion of a residential building site for the purpose of providing additional parking. Access shall be provided across the adjacent lot on which a one- or two-family dwelling exists. The driveway or accessway serving the parking facility on the vacant lot of an expanded residential building site shall be built using one of the types of surfacing required for parking areas in subsection 110-810(b), as it may be amended from time to time.
 - c. Driveways, accessways, and parking areas. These facilities shall not be built in a manner that impairs any easement.
 - d. Setbacks. Any additional parking area on the vacant portion of an expanded residential building site shall be no closer to the front or rear lot lines than 30 feet, and no closer to the exterior side lot line of the vacant lot than 20 feet. The exterior side lot line shall be the lot line directly opposite the common lot line that lies between the two lots comprising an expanded residential building site.
 - e. Natural vegetation. Existing natural vegetation shall not be cleared from the area within the minimum setbacks required in paragraph "d." of this subsection, except to remove hazards or nuisance vegetation limited to exotic species, vines, poisonous plants, and dead or diseased plants, and in accordance with the tree protection requirements of chapter 98, article II of the Deltona Land Development Code as it may be amended from time to time.
 - f. Lot clearing. Lot clearing shall be limited to an area beyond the setbacks specified in paragraph "e." of this subsection that shall not exceed 55 feet in depth measured from the common lot line of the expanded residential building site toward the opposite side lot line, and 50 feet in width. However, clearing to the maximum allowable width and depth shall not be permitted in every instance, but shall be limited to those instances where that is the minimum amount of clearing required to accommodate the types of vehicles being parked on the vacant lot of the expanded residential building site. In each case, clearing shall be limited in width to the minimum needed to accommodate one parking space of nine feet in width for each vehicle proposed to be parked plus a maximum of ten feet on each side to accommodate any visual screening required by paragraph "g." of this section. In each case clearing shall be limited in depth to a depth that equals the length of the vehicle proposed to be parked plus a maximum of an additional ten feet to accommodate any visual screening required by paragraph "g." of this section.
 - g. Visual screening. If any portion of the parking area provided on the vacant portion of an expanded residential building site is visible from any adjacent street or lot, except the portion facing the common lot line of the site, that portion of the parking area must be screened from view. Minimum screening required shall be either a 100 percent opaque hedge, fence or wall at least four feet, but not more than six feet in height, and a row of understory trees high enough that their crowns obscure the parked vehicles from view.

Chain link fences with cover materials or inserts shall not be permitted to meet this screening requirement. The required understory trees shall be planted so that their crown spreads at maturity shall completely cover the area within which any parked vehicle is visible. The required shrubs and understory trees shall reach maturity and achieve the required minimum screening within two years from the date of planting. All required landscaping materials shall be of the species specified in the approved plant species list in section 110-808 of the Land Development Code of the City of Deltona, as it may be amended from time to time. Except that deciduous species that drop their all or most of their leaves at any time of year are prohibited to be used as screening materials meeting the requirements of this section.

- (3) For all other uses, off-site parking and loading areas shall be designed and located according to the requirements of this ordinance and the applicable articles of the Land Development Code, Ordinance No. 96-25, as it may be amended from time to time.
- (f) Minimum off-street parking spaces. Minimum off-street parking spaces shall be provided with adequate means for vehicle ingress and egress from a public street or alley by an automobile of standard size, in accordance with the following table. Fractional spaces shall be rounded to the closest whole number. In stadiums, houses of worship, sports arenas, or other places of assembly where occupants sit on seats without dividing arms, each 18 linear inches of such seat shall be counted as one seat.

The minimum and maximum number of parking spaces required for any use not specifically mentioned, shall be determined by the zoning enforcement official or his or her designee based upon data from the Institute of Transportation Engineers Parking Generation Manual, from publications and data from the American Planning Association or the Urban Land Institute, from studies using ITE recommended methodology and other professionally acceptable sources. Information that other land uses, which are the same as, or similar to, the land use for which a parking determination is sought, have been provided a given number of parking spaces in other jurisdictions shall not be controlling in determining parking requirements, unless such requirements in other jurisdictions are supported by publications, data and information available, or presented in writing, to the zoning enforcement official.

Table 110-9: Minimum Off-Street Parking Spaces

Land Use	Number of Parking Spaces
Parks:	
Open "free play area"	8 spaces per acre.
Equipped playground	10 spaces per site.
Multipurpose court	5 spaces per court.
Picnic area	1 space per table.
Baseball/softball	38 spaces per field.
Handball/racquetball court	2 spaces per court.

Tennis court	2 spaces per court.
Soccer/football	34 spaces per field.
Shuffleboard court	2 spaces per court.
Basketball court	5 spaces per court.
Jogging/fitness trail	2 spaces per trail.
Multipurpose field	8 spaces per acre.
Primitive camping	1 space per site.
Fishing pier	4 spaces per 50 lineal feet.
Boat ramp	36 spaces per boat lane.
Volleyball	6 spaces per court.
Concession building	1 space per concessionaire or employee.
Community center	1 space per 200 square feet GFA.*
Community pool—50 meter (164' × 75')	91 spaces.
Neighborhood pool—25 yards (75' × 45')	25 spaces.
Transportation centers	1 space for each 4 estimated average daily passengers To be addressed in the future—Multi-modal.
Recycling collection center	1 space per employee.
Solid waste transfer station	1 space per employee.
Banks	4 spaces/1,000 square feet of GFA with 5 reservoir spaces per drive thru window and drive thru ATM.

One- and two-family homes	1 BR and more: 2 spaces/d.u., in addition to garage parking, if any.
Multi-family	<p>Studio and 1 BR: 1.5 spaces/d.u. + 1 space per 10 d.u. for guest parking.</p> <p>2 BR: 2 BR or more: 2.0 spaces/d.u. + 1 space per 10 d.u. for guest parking.</p> <p>No recreational vehicles, boats, or trailers are permitted except within an additional visually screened and secured parking area that may be provided specifically for recreational vehicle, boat, and/or trailer storage.</p> <p>Board discussed prohibiting parking on side lots between houses</p> <p>Melissa suggestion to omit, and to allow for off-street parking area for uninhabited recreational, vehicle not to exceed the 19ft requirement in (e)(1)</p>
Hardware store	2.86 spaces /1,000 square feet GFA.
Home improvement superstore	3.5 spaces/1,000 square feet GFA** applicant may increase to 5 spaces/1,000 GFA provided additional spaces may be classified as alternative surface spaces when located greater than 300 feet from the front door.*Garden Center area shall be included.
Hotels, motels	1 space/rental unit 1 space/guest room, plus an additional 10% for employees, plus additional parking required for any other land uses on the site, such as restaurants or bars.
Hospitals	1 space per bed.
Colleges, community colleges, or other places of higher learning	1.25 spaces per student and a minimum of 1 space per 250 square feet GFA of office area up to 1,000 office spaces, and 1 space per 500 GFA of office area for offices over a campus total of 250,000 square feet of GFA office area and 1 space per 200 square feet GFA devoted to classrooms, plus minimum parking required for other areas of assembly, sports arenas, and stadiums.
Nursing and convalescent facilities	1 space/2 patient beds, based on the maximum designed capacity of the facility.

Live theater or auditorium	1 space/3 persons permitted at fire code maximum occupancy plus 10% for employee parking.
Movie theater	1 Screen - 1 space/3 seats permitted at maximum fire code occupancy, plus 10% for employee parking. 2 Screens - 1 space/4 seats permitted at maximum fire code occupancy, plus 10% for employee parking. >2 Screens - 1 space/5 seats permitted at maximum fire code occupancy, plus 10% for employee parking.
Church	1 space/3 seats in main assembly area, or 33.3 spaces/1,000 square feet GFA in main assembly area if no fixed seating is provided. Seating shall be based on maximum fire code occupancy. Plus parking required for other uses on the site that operate during hours when the main assembly area may be in use.
Mortuary or funeral home	1 space/3 seats in main assembly area, or 33.3 spaces/1,000 square feet GFA in main assembly area if no fixed seating is provided. Seating shall be based on maximum fire code occupancy.
Other places of assembly	1 space/3 seats in main assembly area, or 33.3 spaces/1,000 square feet GFA in main assembly area if no fixed seating is provided. Seating shall be based on maximum fire code occupancy.
Restaurants	Type A: Indoor service, low turnover: 12 spaces/1,000 square feet GFA indoor service; High turnover: 14 spaces per 1,000 square feet GFA, plus 6 reservoir spaces/service lane, with a minimum of 3 spaces behind the order station or menu. Type B: Fast food: 6 reservoir spaces/ service lane with a minimum of 3 spaces behind the order station or menu, plus 10 spaces/1,000 square feet GFA. Restaurant without customer seating 1 space/100 square feet GFA.
Bars and night clubs	1 space/100 square feet GFA.
Office buildings less than 25,000 square feet	3.25 spaces/1,000 square feet GFA.
Office buildings between	3.0 spaces/1,000 square feet GFA.

25,000 sq. ft and 500,000 square feet	
Office buildings over 500,000 square feet	Determined by parking demand study using professionally acceptable parking demand methodology approved by the zoning enforcement official, with results to be reviewed and approved by the city, but not less than 3 spaces/1,000 square feet GFA.
Medical offices, clinics, and laboratories	Min: 5 spaces/1,000 square feet GFA. Max: 6 spaces/1,000 square feet GFA.
Retail sales and services, general	Less than 150,000: 4.44 space/1,000 square feet GFA 150,000 square feet or greater: 4.0 space/1,000 square feet GFA. Garden center area shall be included.
Shopping centers	4.0 spaces/1,000 square feet GFA. Garden center area shall be included.
Library	5.0 spaces/1,000 square feet GFA.
Automated service station retail sales (Types A and B)	1 space/gas pump, plus 3 spaces/service bay (not including service bays as parking areas)
Automobile service station with retail sales (Type C)	1 space/gas pump, plus 5 spaces/1,000 square feet GFA.
Government office building	4 spaces/1,000 square feet GFA plus required seating auditorium when an assembly area is included.
Manufacturing	1.54 spaces/1,000 square feet GFA for manufacturing, plus and Industrial Required parking for other uses on the site.
Commercial and industrial dead storage	4 spaces/1,000 square feet.
Contractors storage yards	1 space/1,000 square feet.
General warehouses	1.54 spaces/1,000 square feet GFA, plus 3.5 spaces/1,000 square feet of office or retail area.

Self-service and mini-warehouses	1 space/10 cubicles or units, plus continuous Loading spaces clear of through traffic access, plus 3.5 spaces/1,000 square feet of office area.
Bowling alleys	4 spaces/alley, plus required parking for other uses on the site, plus 10% for employees.
Private clubs and lodges	1 space/3 persons permitted at the maximum fire code capacity of the main assembly area.
Day care center	1 space/state required staff, plus 1 space/5 children at maximum permitted capacity, plus 1 passenger loading space/ea. 10 children under care, minimum 4 spaces. In lieu of the above requirement, a two lane loading and access area may be provided in accordance with the design standards for such loading and access areas in the Deltona Zoning Ordinance, as it may be amended from time to time. When such loading and access area is provided, the minimum parking requirement shall be reduced to 1 space/10 children at maximum licensed occupancy, plus 1 space/state required staff person.
Group home	1 space/5 resident clients, plus 1 space/state Required employee on the largest shift.
Bed & breakfast	1 space/guest room, plus 10% for employee/guest parking, plus 2 spaces/permanent resident dwelling unit.
Ball park or stadium, not including Little League and similar children's recreational programs	1 space/ea. 3 seats or 1/300 square feet GFA, whichever is greater.
Recovery homes	1 space/2 patient beds, plus 10% for staff. If visitation is allowed, add another 10% for visitors.
Pool halls and billiard parlors	2 spaces/pool and billiard table, plus required parking for all other uses on the site, including restaurants or bars.
Golf or country club	6 spaces/golf hole, plus required parking for any other uses on the site.
Swim club	1 space/250 square feet member use area in principal building, plus 1 space/50 square feet of pool and deck area, 1 space per three (3) seats

	in any spectator area, and required parking for any other uses on the site.
Rooming or boarding houses and dormitories	1.5 spaces/rented room or unit, or 1 space/400 square feet boarding houses GFA when dormitory style facilities are provided.
Fraternities or sororities or student cooperatives	1 space/2 occupants based on fire rated capacity of the building.
Mobile home parks	2 spaces/d.u. plus any additional spaces required to service accessory buildings or structures, plus required parking for all other uses on the site.
Car wash	Full service: 1 space per employee on maximum shift plus sufficient area for stacking spaces. Self service: 1 stacking space per washing bay.
Veterinary clinics	4.44 space/1,000 square feet GFA.
Telemarketers	10 space/1,000 square feet GFA.
Furniture stores	2 spaces/1,000 square feet GFA.
Health club and spas	5.71 space/1,000 square feet GFA .
All land uses, maximum permitted parking	Maximum permitted number of parking spaces shall not exceed 1.15 times the minimum parking required by this section, unless otherwise stated herein, or unless a waiver of the maximum parking limitation is obtained from the zoning enforcement official pursuant to this section.

- (g) Minimum requirements for off-street handicapped parking. Except for standard and manufactured single-family dwellings, and two-family standard or manufactured dwellings, where off-street parking spaces are required by this chapter, the number to be reserved for the handicapped shall be determined from the following table.

Table 110-10: Minimum Requirements for Off-Street Handicapped Parking

Total No. of Off-Street Parking Spaces	No. of Spaces Required to be Reserved for Handicapped
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Up to 25	1
26 to 50	2
51 to 75	3
76 to 100	4
101 to 150	5
151 to 200	6
201 to 300	7
301 to 400	8
401 to 500	9
501 to 1,000	2 percent of total
Over 1,000	20 plus 1 for each 100 over 1,000

(h) Off-street loading and unloading regulations. At the time of construction of one of the following categories of buildings, or at the time of structural alteration for an increase in size or capacity, there shall be provided minimum off-street loading or unloading spaces with adequate means of ingress and egress from a public street or alley, without interfering with the public use of streets, or off-street parking spaces. Off-street parking spaces may not be used to meet off-street loading requirements.

The dimensions, design, and location of all off-street loading spaces shall meet the requirements of article IV of the Land Development Code.

The minimum numbers of off-street loading spaces shall be determined from the following table:

Table 110-11: Off-Street Loading Spaces

Use Category	Floor Area in Square Feet	Loading Space Required
Retail sales and service, restaurants or similar uses	3,000—10,000	1

	10,001—20,000	2
	Each additional 20,000 square feet or fraction	1
Offices, hotels, hospitals, nursing homes, assisted living facility, multi-family dwellings or similar uses	30,000—100,000	1
	Each additional 100,000 square feet or fraction	1
Arenas, auditoriums, stadiums, convention centers, exhibition halls, museums or similar uses	10,000—50,000	1
	50,001—100,000	2
	Over 100,000	4
Any industrial use and any wholesale, retail and commercial storage facility	15,000—40,000	1
	40,001—100,000	2
	100,000—160,000	3
	Each additional 80,000 square feet or fractions	1

- (i) Bicycle parking regulations. Each of the following uses shall be required to provide parking spaces for bicycles: parks and recreation areas; convenience stores; restaurants (Types A and B); game rooms; pharmacies; shopping centers (regional, community, and neighborhood); and any employment facility (i.e., office, industrial) with at least 50 employees.

The minimum number of bicycle spaces to be provided shall be determined from the following table:

Table 110-12: Minimum Number of Bicycle Spaces

Required No. of Automobile Parking Spaces	Minimum Number of Required Bicycle Parking Spaces
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1—40	2
41—60	3
61—80	4
81—100	5
Over 100	6 plus 1 for each 20 automobile parking spaces over 100, provided that the maximum number of required bicycle spaces shall not exceed 20.

All bicycle parking shall be located so as to not conflict with automobile or pedestrian traffic flow.

- (j) Mass transit parking requirements. Community and regional shopping centers shall be designed to accommodate buses for convenient and safe boarding and unloading of passengers as well as maintaining a safe traffic pattern. Shopping centers of greater than 100,000 square feet of gross leasable floor area shall provide a passenger shelter or covered benches to accommodate the mass transit system riders. Bus stops, shelters, and benches shall be designed so as to avoid interference with automobile and pedestrian traffic from mass transit operations and facilities.

(Ord. No. 35-00, §§ 1, 2, 11-7-2000; Ord. No. 26-01, § 1, 9-17-2001; Ord. No. 28-01, § 2, 10-15-2001; Ord. No. 06-2003, § 3, 11-3-03; Ord. No. 27-2006, § 1, 11-6-2006; Ord. No. 06-2007, §§ 4, 5, 4-16-2007; Ord. No. 19-2011, § 1(Exh. A), 11-7-2011)