

City of Deltona

2345 Providence Blvd.
Deltona, FL 32725

Agenda

Planning and Zoning Board

Chair Tom Burbank
Vice Chair Adam Walosik
Member Noble Olasimbo
Member Stony Sixma
Member John Harper
Member Donald Philpitt
Member Michael Putkowski

Wednesday, June 15, 2016

7:00 PM

Deltona Commission Chambers

1. CALL TO ORDER:

2. ROLL CALL:

3. APPROVAL OF MINUTES & AGENDA:

A. Minutes for May 18, 2016

4. PRESENTATIONS/AWARDS/REPORTS:

5. PUBLIC FORUM:

6. OLD BUSINESS:

A. [Ordinance No. 24-2016: Project No. RZ15-006, Vineland Reserve, Residential Planned Unit Development](#)

7. NEW BUSINESS:

B. [Ordinance No. 26-2016: Project No. RZ16-001, Deltona Free Standing Emergency Room, Business Planned Unit Development](#)

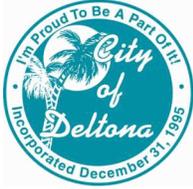
C. [Ordinance No. 23-2016: Amending Section 110-828, Off-street Parking and Loading Land Development Code Regulations, Pertaining to Single Family Residential Lots](#)

8. STAFF COMMENTS:

9. BOARD/COMMITTEE MEMBERS COMMENTS:**10. ADJOURNMENT:**

NOTE: If any person decides to appeal any decision made by the City Commission with respect to any matter considered at this meeting or hearing, he/she will need a record of the proceedings, and for such purpose he/she may need to ensure that a verbatim record of the proceedings is made, which record includes the testimony and evidence upon which the appeal is to be based (F.S. 286.0105).

Individuals with disabilities needing assistance to participate in any of these proceedings should contact the City Clerk, Joyce Raftery 48 hours in advance of the meeting date and time at (386) 878-8500.



City of Deltona

2345 Providence Blvd.
Deltona, FL 32725

DRAFT Minutes - Draft Planning and Zoning Board

Wednesday, May 18, 2016

7:00 PM

Deltona Commission Chambers

1. CALL TO ORDER:

2. ROLL CALL:

Also present: Ron Paradise, Planning and Development Services; Scott McGrath, Planning and Development Services and Kathrine Kyp, Planning and Development Services.

Present: 7 - Chair Tom Burbank
Vice Chair Adam Walosik
Member John Harper
Member Noble Olasimbo
Member Donald Philpitt
Member Michael Putkowski
Member Stony Sixma

C. Project No. RZ15-006, Ordinance No. 24-2016, Vineland Reserve

Chairman Burbank stated that per the request of the applicant, this item will be continued to a date certain of June 15, 2016 at 7:00 pm and will be heard by the Planning and Zoning Board at that time.

There were concerns from residents in attendance that if they were not available to attend the June 15, 2016 Planning and Zoning Board meeting. The Chairman informed them that that they could submit a letter to be read in at that meeting.

3. APPROVAL OF MINUTES & AGENDA:

A. Minutes for March 16, 2016

Motion by Member Putkowski, seconded by Member Olasimbo, to approve the minutes for March 16, 2016. The motion carried by the following vote:

For: 7 - Chair Burbank, Vice Chair Walosik, Member Harper, Member Olasimbo, Member Philpitt, Member Putkowski and Member Sixma

4. PRESENTATIONS/AWARDS/REPORTS:

5. PUBLIC FORUM:**6. OLD BUSINESS:****7. NEW BUSINESS:****A. Comprehensive Plan Amendment adding Tivoli Dr. between Saxon Blvd. and Providence Blvd. to the City's Thoroughfare Map as a principal arterial and recognizing said segment of Tivoli Dr. as a future four lane facility. Ordinance No. 12-2016.**

Mr. Paradise gave summary of comprehensive plan amendment and why it is being revisited by the Board. He noted that it was not advertised properly which brings it back to the Board for review. Mr. Paradise stated that Ordinance No. 12-2016 is a request to add a section of Tivoli Dr. between Saxon Blvd. and Providence Blvd. to the City's Thoroughfare Map as a principal arterial and recognizing said segment of Tivoli Dr. as a future four (4) lane facility. He also noted that this does not obligate the City to improve it to a four (4) lane road, but allows the City to potentially improve it to that standard, if necessary. He stated that the City hired a consultant to study this segment of the road and suggested an incremental approach for managing traffic by starting with improving the Tivoli Dr. and Providence Blvd. intersection and the Tivoli Dr. and Saxon Blvd. intersection.

Chairman Burbank opened the public hearing.

David Grant, Sr., 1379 Tivoli Dr., Deltona, spoke against the item and noted the existing high speed traffic on Tivoli Drive and widening would increase the speed on the road even more. He noted with the roadway widening it would mean he would be stepping out of his house onto the street which would be dangerous.

Richard Klein, 1541 Laramore St., Deltona, spoke against the item and noted widening of the road would cause more of a traffic backup if improvements are not made to surrounding streets and asked if anyone contacted the County for the widening Providence Blvd. or Saxon Blvd.

Shawna Young, 1425 Lydia Dr., Deltona, spoke about impacts associated with the Saxon Blvd. widening and is against the four lanes in her backyard. She asked if the traffic counts represent the people who reside on the street and the surrounding neighborhoods. She also spoke regarding the potential decrease in property values.

Member Philpitt stated that there is no mention of intersection improvements within the ordinance or staff report only to improve the road to four lanes.

Member Walosik noted this policy is for future improvements and road construction will cost a lot of money. The policy puts it in place when the improvements are needed.

Member Walosik spoke about his 28 years of driving this road and noted improvements need to begin with the intersections. He noted the City will not take any property but use existing road of right of way.

Member Olasimbo stated this is a policy action to add the segment of Tivoli Dr. to the thoroughfare map to allow the City the ability to apply for transportation improvement funds.

Member Walosik noted that any construction improvements will utilize existing road right of way and not cut into resident's yards.

Discussion amongst the board occurred regarding nearby schools, student safety, maneuverability of the road, difficult home sales on arterial roads and how Google Maps' directions direct traffic onto Tivoli Dr.

Motion by Member Olasimbo to Comprehensive Plan Amendment adding Tivoli Dr. between Saxon Blvd. and Providence Blvd. to the City's Thoroughfare Map as a principal arterial and recognizing said segment of Tivoli Dr. as a future four lane facility. Ordinance No. 12-2016. The motion failed for lack of second.

Discussion amongst the board occurred to modify the ordinance with language to state that road construction could occur incrementally. Mr. Paradise noted that the board could add language noting Tivoli Dr. as a thoroughfare with options to keep the lanes at two (2), increase to three (3) or four (4) lanes.

Motion by Member Olasimbo to Comprehensive Plan Amendment adding Tivoli Dr. between Saxon Blvd. and Providence Blvd. to the City's Thoroughfare Map as a principal arterial and recognizing said segment of Tivoli Dr. as a future three lane facility. Ordinance No. 12-2016. The motion failed by the following vote:

For: 3 - Chair Burbank, Vice Chair Walosik and Member Olasimbo

Against: 4 - Member Harper, Member Philpitt, Member Putkowski and Member Sixma

B. VR16-002 Fence Variance - 3041 Bond St. Resolution 2016-21.

Mr. McGrath provided a brief description regarding the request for the fence variance. He stated the applicant has a special needs dependent that is physically and mentally

disabled. The request is for a variance for the location of the fence to be located at the property boundary, instead of along the building setback line within a side street yard. As requested, the proposed six (6) foot high fence would provide an extra level of safety and privacy for their special needs dependent. Discussion between Mr. McGrath and the board occurred regarding location and sight triangle consideration.

Angela Owen, 3041 Bond St., Deltona, noted the request for the fence variance is because people were walking through her yard, stopping to ask her son to use the restroom and request a glass a water.

Carl Bolli Jr, 3033 Bond St., Deltona, spoke for the application.

Motion by Member Harper, seconded by Member Putkowski, to recommend for approval, VR16-002 Fence Variance - 3041 Bond St. Resolution 2016-21. The motion carried by the following vote:

For: 7 - Chair Burbank, Vice Chair Walosik, Member Harper, Member Olasimbo, Member Philpitt, Member Putkowski and Member Sixma

D. VR16-001 Fence Variance - 1690 Panama Court. Resolution 2016-20.

Mr. McGrath gave a summary regarding the variance request to build a five (5) foot fence on the property line.

Chairman Burbank and Mr. McGrath discussed why a five (5) foot fence versus a four (4) foot fence. Mr. McGrath noted that a five foot fence would add consistency to the remaining houses on the cul-de-sac.

Jesse Windam spoke on behalf of Linda Hodges, 1690 Panama Ct., Deltona - General Contractor spoke on behalf of the applicant. He noted they originally asked for a six (6) foot fence and then reduced it to five (5) feet. Furthermore, the fence would be installed to not obstruct the view and enhance the neighborhood.

Chairman Burbank did not see the hardship for the variance.

Motion by Member Olasimbo, seconded by Member Philpitt, to recommend for approval, VR16-001 Fence Variance - 1690 Panama Court. Resolution 2016-20. The motion failed by the following vote:

For: 3 - Member Olasimbo, Member Philpitt and Member Putkowski

Against: 4 - Chair Burbank, Vice Chair Walosik, Member Harper and Member Sixma

8. STAFF COMMENTS:

9. BOARD/COMMITTEE MEMBERS COMMENTS:

10. ADJOURNMENT:

The meeting adjourned at 7:47 pm.

Tom Burbank, Chairperson

ATTEST:

Kathrine Kyp, Board Secretary



Staff Report

To: Planning and Zoning Board
From: Chris Bowley, AICP, Director of Planning and Development Services
Date: May 4, 2016
Re: Project No. RZ15-006, Ordinance No. 24-2016, Vineland Reserve

A. Summary of Application:

Applicant: Michael J. Woods, Esq. representing Lake Disston Lands, LLC

Request: Rezone multiple parcels totaling approximately 102.7 acres from Residential Planned Unit Development (RPUD) to RPUD

Tax Parcel No.: 9112-00-00-0062, 0020, 0030, 0070, 0150, 0250, 0151, 0251, and 0061

Property Acreage: ± 102.7Acres

Property Location: The property is located in the southeastern section of the City between Doyle Road and Collins Road west of SR 415 near Osteen Elementary School.

Legal Description: See attached.

B. Existing Zoning: Residential Planned Unit Development (RPUD)

C. Background: The subject property was annexed into the City in 2004 and was rezoned in 2007 to a Residential Planned Unit Development (RPUD). The 2007 RPUD approved under Ordinance No. 22-2007 featured 346 residential dwelling units. Of the 346 dwelling units, 128 were proposed to be multi-family townhomes. Notwithstanding the City approving Ordinance No. 22-2007, the applicant/developer never executed the Development Agreement. Shortly thereafter, the property went into receivership. Eventually, the property was bought by Lake Disston, LLC and an application has been made to the City to rezone the property. The new RPUD proposal is similar to the 2007 development plan. However, the number of units requested has increased from 346 to 407. Of the 407 units proposed 279 are planned to be detached single family lots and the remaining 128 are townhome multi-family

units. The overall density is a little under four units per acre. Note: when the property was annexed, the City never changed the County Future Land Use Designation of Urban Low Intensity (ULI). Under F.S. 171.062, County Future Land Use Designations remain in effect for annexed land until a City changes the land use. In the case of the Vineland Reserve project, the development is still consistent with the County ULI designation. Finally, there is an inholding within the property proposed for rezoning. The inholding is a cell tower site that is not owned by the applicant.

D. Support Information

Public Facilities:

- a. Potable Water: Deltona Water
- b. Sanitary Sewer: Deltona Water
- c. Reuse Water: Deltona Water
- d. Fire Protection: Deltona Fire. Station 64 is the closest City fire station
- e. Law Enforcement: Volusia County Sheriff's Office (VCSO)
- f. Electricity: Florida Power and Light

E. Matters for Consideration:

Section 110-1101, Code of City Ordinances, states that the City shall consider the following matters when reviewing applications for amendments to the Official Zoning Map:

1. Whether it is consistent with all adopted elements of the Comprehensive Plan.

As has been stated, the property is designated with a County Future Land Use category – Urban Low Intensity (ULI). According to State Law (F.S. 171) there is no preclusion of a municipality utilizing a County Future Land Use classification to approve zoning and/or land development proposals. According to the County Comprehensive Plan, the density range for the ULI is 0.2 to 4 dwelling units per acre. The 407 units proposed on the 102.7 acres of land is just below the 4 dwelling units per acre threshold and within the density range of the ULI.

The most comparable City Land Use designation to the County ULI is Low Density Residential (LDR). The LDR Land Use designation has a density range of 0 to 6 dwelling units per acre and recognizing the suburban nature of the City is the most widely applied Land Use designation. Therefore, the requested four dwelling units per acre density is well within the range of the City LDR designation and comports with City density expectations as articulated within the City Comprehensive Plan. Finally, the LDR designation while recognizing the dominate detached single family dwelling unit on an individual lot development pattern, still allows for a range of housing types including attached units like the townhomes planned.

The following Comprehensive Plan provisions are applicable in light of the RPUD rezoning request:

Policy FLUI-1.1

Development consistent with the Future Land Use Map shall not occur until services and facilities have been determined to be available concurrent with the impacts of the proposed development.

9J-5.006(3)(c)(1,3,6)

There is adequate public service capacity to support the Vineland RPUD at the density proposed. Public services include central water and sewer, transportation and school workstation capacity.

Policy FLUI-1.5

Densities or intensified new development shall not exceed the capacity of the existing transportation system or the capacity of improvements as programmed in the Transportation and Capital Improvements elements unless the City proceeds with implementing a transportation concurrency exception area (TCEA).

9J-5.006(3)(c)(3,4,7)

The applicant has submitted a traffic impact analysis which indicates the City transportation network has adequate capacity to service the 3,497 trips generated from the project. However, there have been improvements identified to facilitate safe and efficient flow of traffic. The improvements include both left and right turn lanes associated with the intersection of the project entrance road and Doyle Road. The owner/applicant will be required to finance and construct the aforementioned improvements. The City did have the transportation findings reviewed and the City review determined the applicant traffic impact analysis and related improvements were appropriate. Finally, none of the traffic modeling and related review was predicated on the project utilizing Collins Road as access to SR 415.

OBJECTIVE FLUI-5

The City of Deltona shall protect natural, archaeological, and historic resources from any adverse development impacts. This will be accomplished through the implementation of the land development regulations and coordination with appropriate permitting agencies.

9J-5.006(3)(b)(4)

The property proposed for development has historically been used for agricultural production including citrus, hay and cattle grazing. Soils on site are sandy and well drained. None of the property is located within the 100 year floodplain. Therefore, the property is suitable to support development. The property does support gopher tortoises.

Tortoises will need to be protected on site and/or relocated as per State permitting requirements.

Policy FLUI-7.9

The residential density guidelines for each Future Land Use category represent an acceptable range and the allowable density shall be based upon the following minimum criteria:

- a. Reducing sprawl by providing options for higher residential densities in appropriate locations;*
- b. Environmental constraints, as established in the Conservation Element;*
- c. Land use compatibility;*
- d. Availability of public facilities and services at acceptable levels of service;*
- e. Character of an area;*
- f. Energy efficient design such as the provision of pedestrian and transit oriented access and options, and the use of efficient subdivision and construction standards;*
and
- g. Other policies of this Comprehensive Plan or Land Development Code, which establish more stringent density requirements.*

The proposed density range of 4 units per acre is well within the City land use density guidelines for residential development (Low Density Residential – 0 to 6 units per acre). In addition, the proposed townhome format and smaller lot sizes represent a more compact development pattern and a more efficient use of land.

Policy FLUI-7.22

The City of Deltona shall use and enforce appropriate Volusia County density/intensity standards on annexed properties, as an interim density/intensity limit, until such time the City Commission amends the City's Comprehensive Plan to apply City land uses to such annexed lands. Such City density/intensity standards shall be consistent with the City's Comprehensive Plan.

As has been mentioned, the City is processing the RPUD rezoning under the County Urban Low Intensity (ULI) Land Use category. The RPUD density, as proposed, is consistent with the County ULI designation. Therefore, the RPUD density is consistent with the above referenced policy.

Policy FLUI-7.13

New residential development shall afford a buffer to non-residential land uses. 9J-5.006(3)(c)(2)

The project is located in the southeastern section of the City near SR 415 and Osteen. The RPUD property has traditionally been on the eastern periphery of the Deltona Lakes plat.

Some of the existing land uses in the vicinity of the Vineland RPUD have been more rural in character including large lot subdivisions accessed by unpaved roads. However, notwithstanding some of the rural elements in the area, the RPUD property and surrounding area have been earmarked through both the County and City of Deltona planning programs for urban uses. Even though there is an urban expectation for the area articulated by both the City and County Comprehensive Plans, compatibility is important. Consistent with the above referenced policy, buffers will be used to provide an appropriate transition between the proposed urban development and other less urban land uses in the vicinity.

Policy FLUI-8.1

The Future Land Use Map shall contain adequate residential land to accommodate the projected population. 9J-5.006(3)(c)(1,2,7)

City population projections are illustrated in the below table:

Year	2010	2015	2020	2025
Population	85,182	87,367	92,995	98,018
Source: Shimberg Center for Housing, University of Florida				

The table above clearly indicates population growth within the City within the current planning window. In fact, there is an approximately 11% population increase forecasted between 2015 and 2025. There is anticipation the City can accommodate the expected 2025 population with existing vacant land resources. The 102.7 acre tract is part of the City vacant land inventory and the rezoning request represents an efficient use of land. In addition, the Vineland project, as proposed, will ensure the City can still grow without having to expand its jurisdictional borders into rural and/or ecologically sensitive areas away from public services.

2. Its impact upon the environment or natural resources.

The majority of the property has been or is being used for agricultural endeavors. Historically, portions of the property were planted in citrus and produced hay. Currently the property is used for cattle grazing. The approximately west 425 feet of the property south of Collins Road is natural. The natural area is forested with a mixture of pines and oaks. The soils on site are well drained and sandy. None of the property is located within the 100 year floodplain. There are no wetlands on site. Topography can be described as gentle and rolling. However there is a depression located along the southern end of the property. The declivity accounts for about 1.5 acres of land. The property does support a population of gopher tortoises. The site will need to be surveyed for tortoises before land development to determine the

tortoise population and the location of burrows. The options to manage gopher tortoises include:

- 1) On-site preservation;
- 2) Relocation; or
- 3) A combination of methods.

All tortoise management will need to be permitted with the Florida Fish and Wildlife Conservation Commission.

3. Its impact upon the economy of any affected area.

The project is a residential development of the property will result in short term construction jobs only.

4. Notwithstanding the provisions of Article XIV of the Land Development Code, Ordinance No. 92-25 [Chapter 86, Code of Ordinances] as it may be amended from time to time, its impact upon necessary governmental services, such as schools, sewage disposal, potable water, drainage, fire and police protection, solid waste or transportation systems.

a. Schools:

The applicant will be required to work with the Volusia County School District with regard to workstation spaces. The Owner may be required by the School District to mitigate if there is a workstation deficiency identified. Currently, there is high school and elementary school capacity within southwest Volusia. However, middle school capacity may be deficient. Staff will have an update on the status of school workstation space and any actions required of the Owner, if any, to address lack of school space.

b. Sewage Disposal:

The project will generate about 115,588 gallons of wastewater a day. The wastewater will be treated at the City's new eastern wastewater treatment plant by Deltona Water. There is plenty of treatment capacity available to treat wastewater flows from the project.

c. Potable Water:

The demand for potable water will be about 122,100 gallons a day. Deltona Water will serve the development and has both the physical and permitted capacity to provide potable water.

d. Drainage:

Stormwater management will be required to comply with St. Johns River Water Management District permitting requirements and the City Land Development Code.

e. Transportation Systems:

The project will generate 3,497 new trips on the City transportation network. These trips are forecasted to enter and exit exclusively onto Doyle Rd. While there are no roadway segments or intersections the project will cause to fail, there are improvements needed to ensure appropriate roadway function. More specifically, turn lanes off of Doyle Rd. to the project entrance road. The turn lanes will ensure cars entering the project will not impede the Doyle Rd. travel lanes. In addition, the design speed of the subject segment of Doyle Rd. warrants turn lanes of sufficient length and design to promote safe and efficient traffic flow. Therefore, at minimum, an eastbound left turn lane of 235 feet (both storage and transition) from Doyle Road to the project entrance road and a 185 foot (both storage and transition) right turn lane off of Doyle Rd. to the project entrance are suggested by City Staff. In addition, the travel lanes associated with the segment of Doyle Rd. where the site is located, will need to be modified to accommodate the turn lanes. The modification of the travel lane alignment needs to occur in a manner where traffic flows in a smooth, seamless manner.

Vineland Reserve is planned with access points to Collins Road. Collins Rd is not designed or improved to a standard to support traffic that will be associated with Vineland Reserve. Therefore, the access points to Collins Rd. will be gated and at this point reserved for emergency access only. If in the future Collins Rd. is improved to a paved, urban street standard, general access for Vineland Reserve will be granted to Collins Rd.

5. Any changes in circumstances or conditions affecting the area.

None.

6. Any mistakes in the original classification.

No known mistakes.

7. Its effect upon the public health, welfare, safety, or morals.

All planned unit development zoning classifications are associated with a written Development Agreement (DA). A DA is intended to address a myriad of development related issues including but not limited to dimensional requirements, density/intensity, traffic, aesthetics, infrastructure etc.

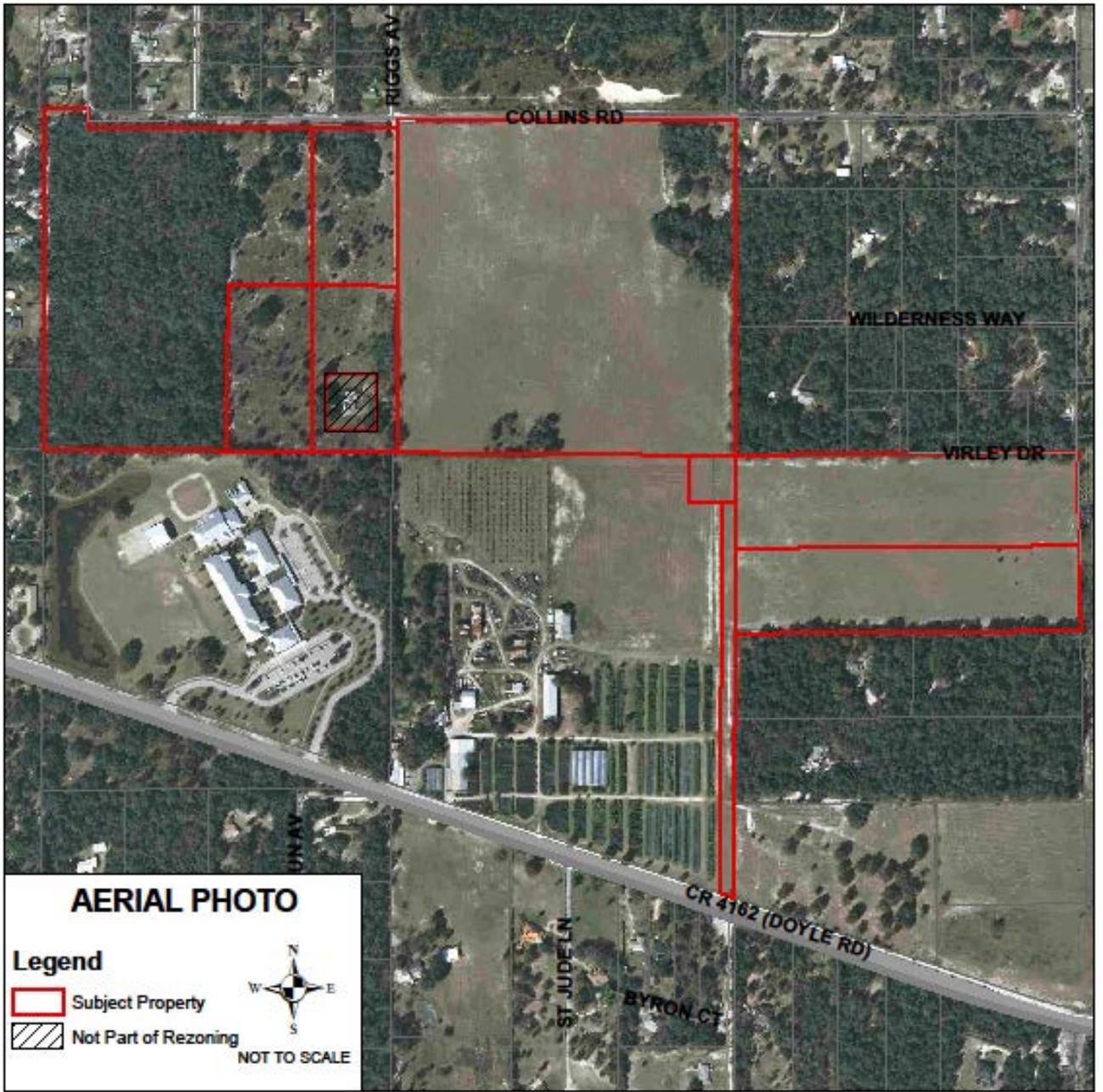
The DA for Vineland Reserve has been reviewed by City staff and staff has suggested changes to the document. The goal is to ensure development within the project is well designed and functional. Major changes to the DA recommended by staff involve the following:

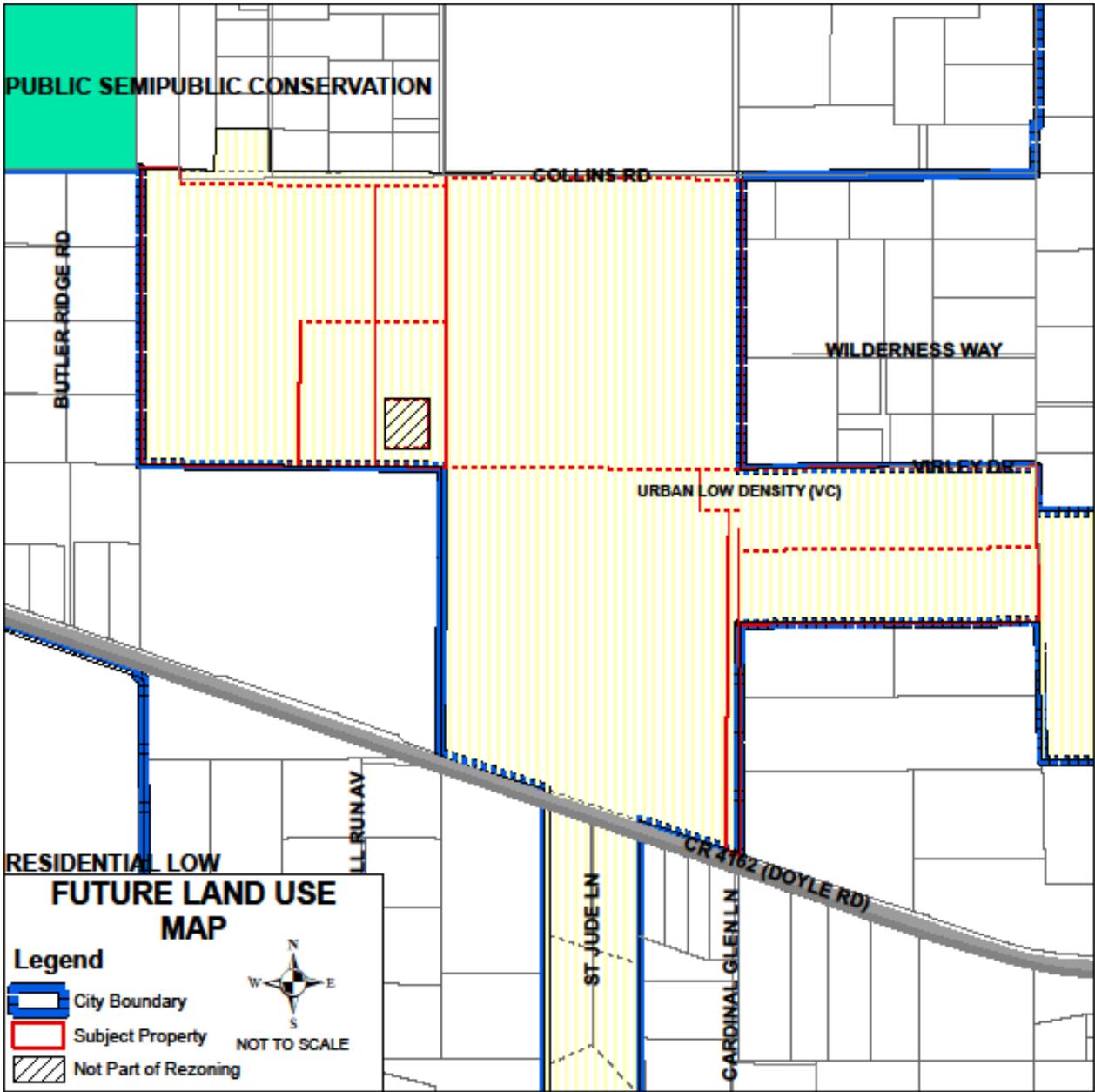
- 1) Housing types and related amenities;
- 2) Entrance signage;
- 3) Dimensional requirements;
- 4) Transportation improvements;
- 5) Common open space;
- 6) Public facilities; and
- 7) Recognizing the City of Deltona Cell tower separation distance and fall radius requirements

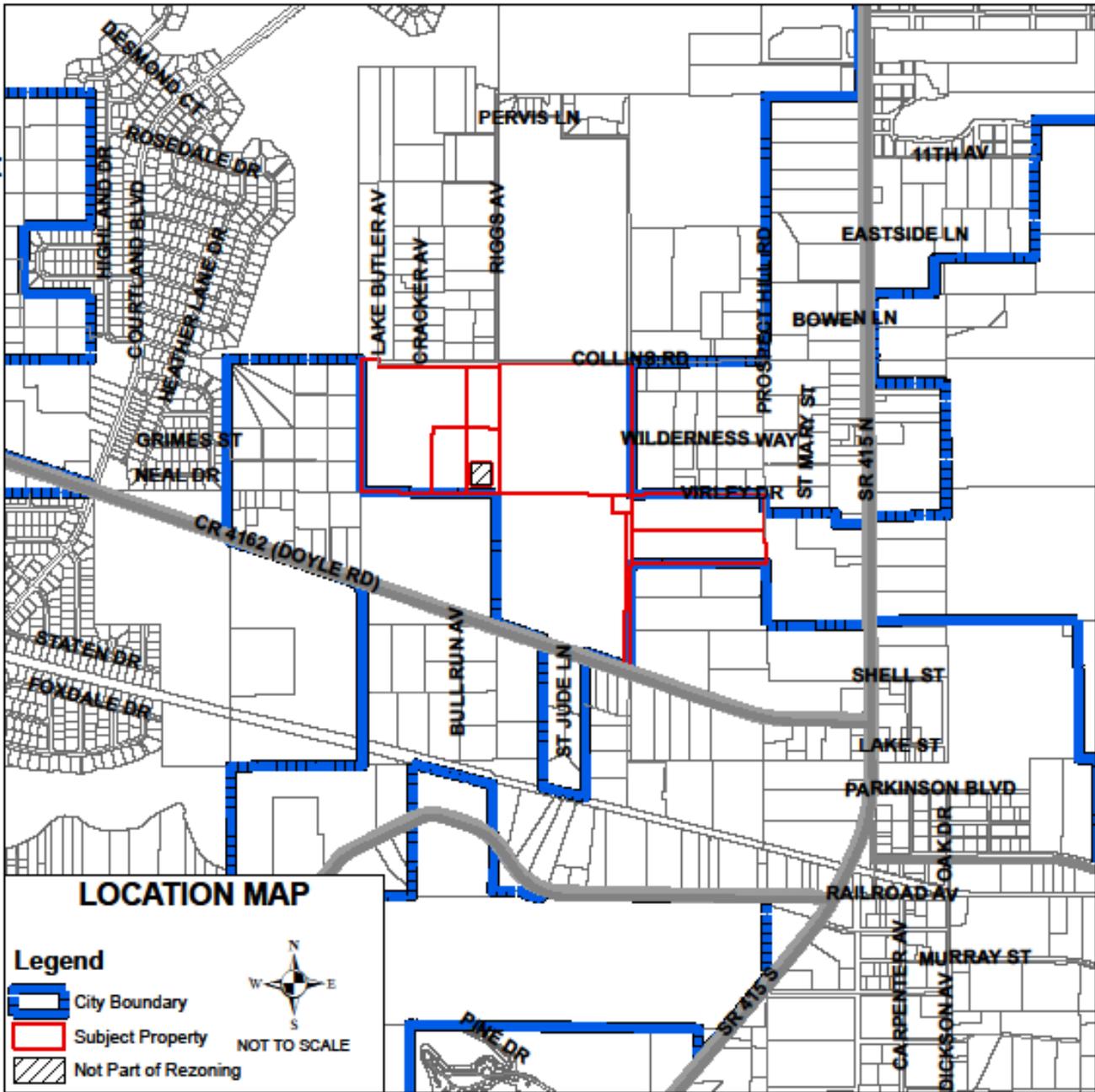
For more information regarding staff recommended changes to the DA, see the attached DA presented in an underline and strike through format.

CONCLUSION/STAFF RECOMMENDATION:

The project is residential oriented. While the majority of the project is comprised of single family lots a portion of the project is proposed for townhome type development. The mixture of housing types within one project is not common within the City. The townhome format provides a housing product that would appeal to buyers not interested in yard work and other maintenance activities typically associated with detached dwellings on individual lots. The Vineland Reserve project, when compared to the Deltona Lakes plat, is a more dense development product. However, density and development quality are not mutually exclusive concepts. The project will have amenities such as a club house, buffers and a forested open space area. In addition, the developed forms within the project will be subject to aesthetic standards. A homeowners association required for the project is anticipated to promote long term quality within the development. From a planning perspective, Vineland Reserve, as proposed, represents a more compact development pattern and an efficient use of existing land resources within the City. Therefore, City staff recommends approval of Ordinance No. 24-2016 with the staff suggested changes to the Development Agreement.





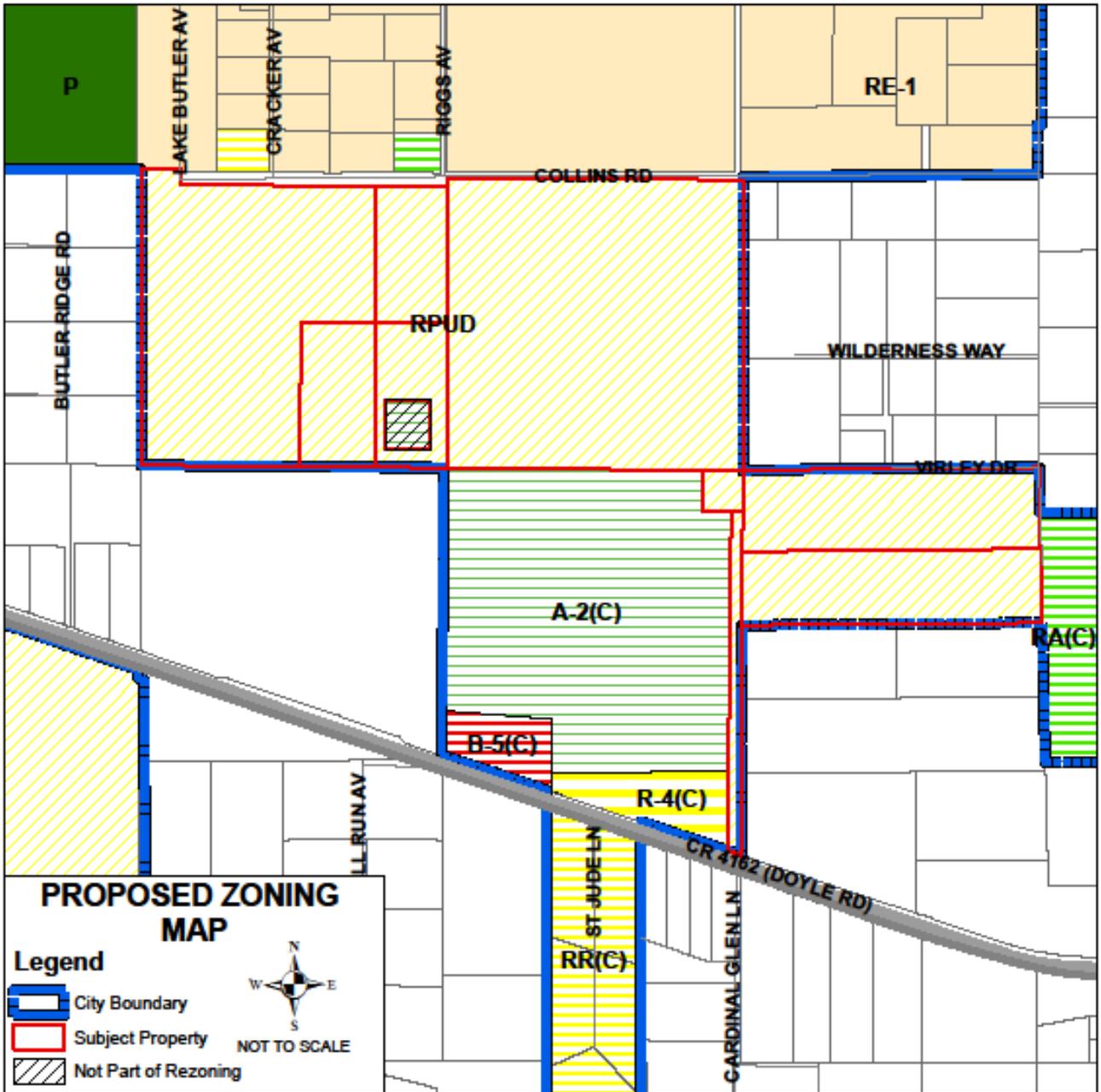


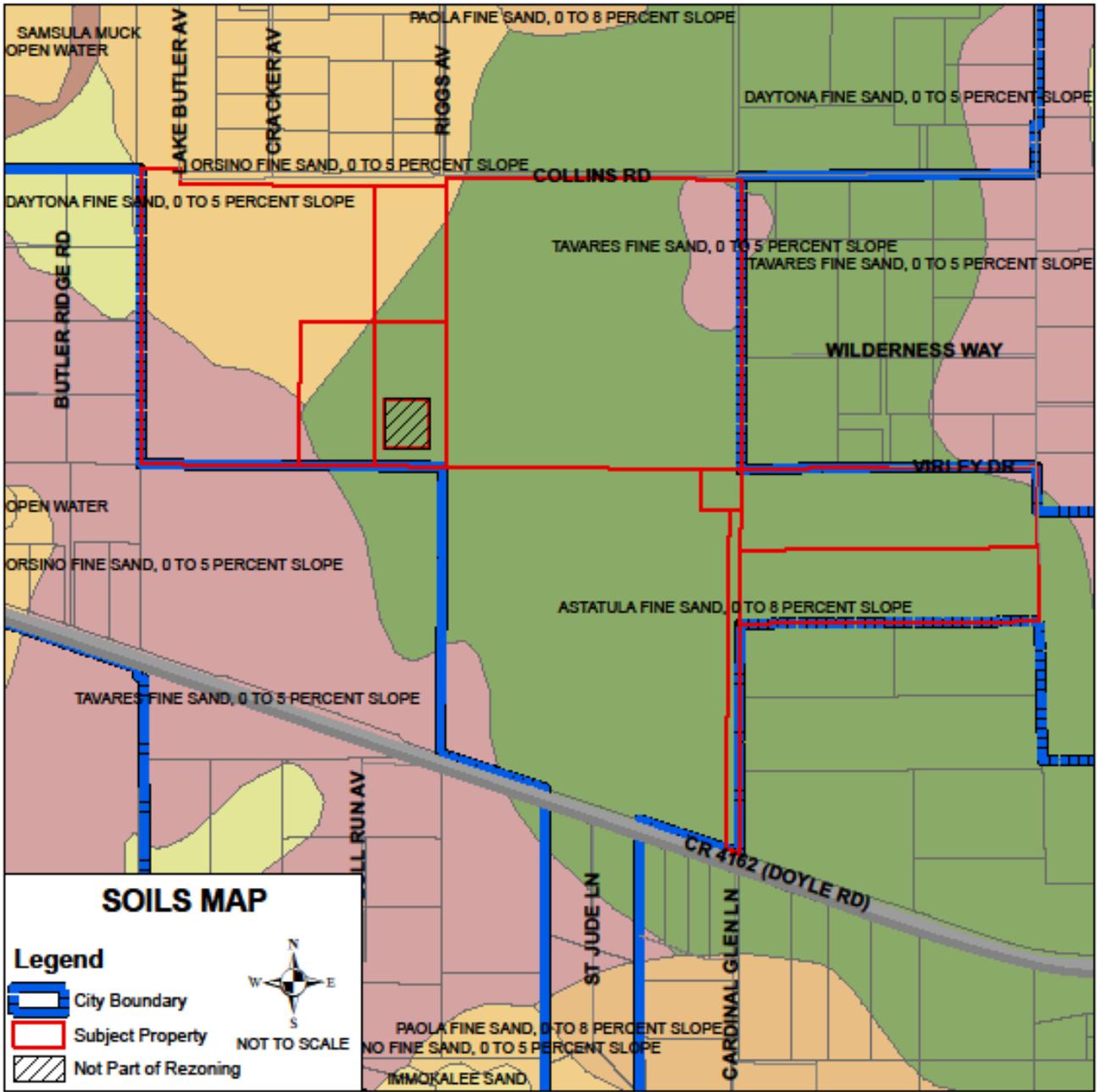
LOCATION MAP

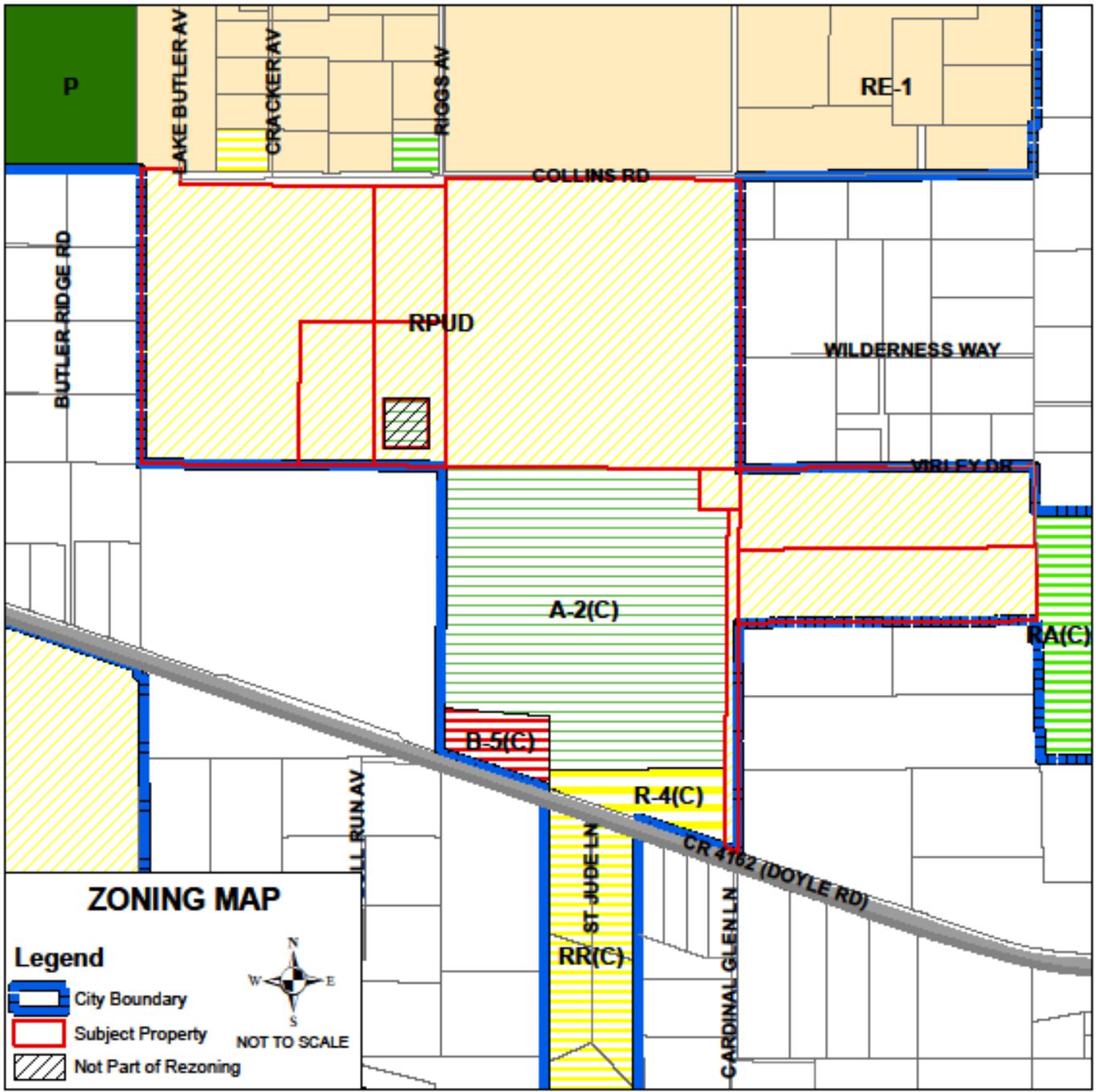
Legend

-  City Boundary
-  Subject Property
-  Not Part of Rezoning

 NOT TO SCALE







**THIS INSTRUMENT PREPARED BY
AND AFTER RECORDING RETURN TO:**

Gretchen R. H. Vose, Esq.
City Attorney
City of Deltona
2345 Providence Boulevard
Deltona, Florida 32725

<p>For Recording Purposes Only</p> <p>Signature of Notary</p>
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Exhibit "A" to Ordinance No. 24-2016

DEVELOPMENT AGREEMENT

for the project known as Vineland Reserve Planned Unit Development (PUD) located at 450 Doyle Road (FKA Pell Place and Vineyard Reserve) (hereinafter referred to as the "Subject Property").

THIS DEVELOPMENT AGREEMENT (hereinafter referred to as the "Agreement") is entered into and made as of the ___ day of _____, 2016, by and between the CITY OF DELTONA, a Florida municipal corporation, with a mailing address of 2345 Providence Boulevard, Deltona, Florida 32725, (hereinafter referred to as the "City"), and Lake Disston Lands LLC, a Florida Limited Liability Company with a mailing address of 230 North Woodland Boulevard, Suite 304, DeLand, Florida 32720 (hereinafter referred to as the "Owner").

WITNESSETH

WHEREAS, the Owner warrants that it holds legal title to the lands located in Volusia County, Florida, and within the corporate limits of the City of Deltona, said lands being more particularly described in Exhibit "B", Legal Description for the Subject Property, attached hereto and by this reference made a part hereof; and that the holders of any and all liens and encumbrances affecting such property will subordinate their interests to this Agreement; and

WHEREAS, the Owner has clear title of the Subject Property; and

WHEREAS, the Owner desires to facilitate the orderly development of the Subject Property in compliance with the laws and regulations of the City and of other governmental authorities, and the Owner desires to ensure that its development is compatible with other properties in the area and planned traffic patterns; and

WHEREAS, the development permitted or proposed under this Development Agreement is consistent with the City's Comprehensive Plan, concurrency management system, and all land development regulations and this Agreement does not replace, supersede, or grant variances to those regulations; and

WHEREAS, it is the purpose of this Agreement to clearly set forth the understanding and agreement of the parties concerning the matters contained herein; and

WHEREAS, the Owner has sought the City's approval to develop the Subject Property, and the City approved Ordinance No. 24-2016, through rezoning the Subject Property to a ~~form~~ of Residential Planned Unit Development (RPUD), as defined under the City's Land Development Code on _____. The RPUD shall consist of this Agreement as the Written Agreement of the RPUD and an Exhibit "C", Master Development Plan (MDP), attached hereto and by this reference made a part hereof as the Preliminary Plan, subject to the covenants, restrictions, and easements offered by the Owner and contained herein, (hereinafter the "Master Development Plan"). Where more detailed criteria for City required submittals exceed the criteria required for a Master Development Plan, the more detailed criteria applies.

NOW THEREFORE, in consideration of the mutual covenants and agreements contained herein, and other good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged, the parties hereto agree as follows:

1. **Recitals and Definitions.** The recitals herein contained are true and correct and are incorporated herein by reference. All capitalized terms not otherwise defined herein shall be as defined or described in the City's Land Development Code as it may be amended from time to time, unless otherwise indicated.

2. **Ownership.** The legal and equitable owner of the Subject Property is: Lake Disston Lands LLC., a Florida Limited Liability Company.

3. **Title Opinion/Certification.** The Owner will provide to the City, in advance of the City's execution and recordation of this Agreement, a title opinion from a licensed attorney in the state of Florida, or a certification by an abstractor or title company authorized to do business in the state of Florida, verifying marketable title to the Subject Property to be in the name of the Owner and any and all liens, mortgages, and other encumbrances that are either satisfied or not

satisfied or released of record.

4. **Subordination/Joinder.** Unless otherwise agreed to by the City and if applicable, all liens, mortgages, and other encumbrances ~~that are~~ not satisfied or released of record, must be subordinated to the terms of this Agreement or the Lienholder join in this Agreement. It shall be the responsibility of the Owner to promptly obtain the said subordination or joinder, in form and substance that is acceptable to the City Attorney, prior to the execution and recordation of this Agreement.

5. **Duration.** The duration of this Agreement is binding and runs with the land in perpetuity, unless amended.

6. **Development of the Subject Property.** Development of the Subject Property shall be subject to performance standards listed in this Agreement. Where a land use listed below differs from a defined use in the City of Deltona's Code of Ordinances, the use listed in this Agreement shall prevail.

- A. The Vineland Reserve RPUD is consistent with the Comprehensive Plan Policies specific to this Subject Property.
- B. Permitted principal uses allowable on the Subject Property:
 - 1a. Residential – Single Family Residential, detached, not to exceed 279 units;
 - 1b. Residential – Townhomes not to exceed 128 units;
 - 2. Residential – Alternative to Townhome Development: The MDP contemplates a section of Townhomes within the eastern portion of the Subject Property. In lieu of the 128 Townhomes illustrated on the MDP, Owners may develop ~~an~~ additional ~~84~~ Single Family Residential lots. However, consistent with Sec. 110-319(k)(5)d of the City Land Development Code, the change shall be processed via a ~~minor~~ major amendment to the this Agreement. and all If the major amendment is approved, all Single Family Residential lots shall be subject to the development criteria provided herein. If Owner avails itself to this option, the total number of Single Family Residential lots for the Subject Property shall not exceed 360 units;
 - 3. Amenity Center Serving Vineland Reserve residents may shall be provided and will consist, at minimum, of the following: swimming pool, club house of no less than 2,000 square feet of climate controlled space; related decking to integrate the pool with the club house, and other amenities as deemed appropriate by the owner/developer. Parking shall be afforded in accordance with section M of this Agreement. All structures and amenities shall be setback at minimum 20 feet from property or lot lines;
 - 4. Home Occupations, as restricted by Section 110-807 of the City's Land Development Code as it may be amended from time to time;
 - 5. ~~Temporary~~ Signage – All signs shall be consistent with Chapter 102 as it may be

amended from time to time. According to Sec. 102-76, subdivision entrance signs can be administratively approved. The main entrance to Vineland Reserve is planned to be off of Doyle Road. Therefore, the City shall allow entrance signage at the Doyle Rd. driveway cut. The entrance sign shall be a free standing monument sign of no more than six feet high with an 18 inch base. The copy area shall feature no more than 75 square feet. The sign shall not contain any electronic messaging. Foundation landscaping will be a required element of the sign. The entrance sign shall be permitted separately and must be approved by the Director of Planning and Development Services. An elevation of the sign, including color renditions, landscaping and other design elements shall be part of the submittal to the Director; and A two-sided sign, with a copy area on each side not to exceed 100 square feet, shall be permitted at the intersection of Doyle Road and the main spine road servicing the Subject Property, as depicted on the Master Development Plan. The sign shall be utilized to direct home buyers to the Subject Property from Doyle Road and shall be permitted so long as a builder or master developer has homes for sale on the Subject Property; and

6. Infrastructure and support uses for above listed uses.

C. Prohibited principal uses, if any:

1. ~~THERE ARE NO EXPRESSLY PROHIBITED USES.~~ Uses that are not customary residential;
2. Mobile/manufactured homes defined by the United States Department of Housing and Urban Development.

D. Proposed minimum density (~~in number of dwelling units per acre~~): ~~0~~ 3 units/ac

E. Proposed maximum density (~~in number of dwelling units per acre~~): 4 units/ac

F. Impervious surface ratio is not to exceed ~~70~~60% of the gross square footage for the Subject Property.

G. Maximum lot coverage (~~in %~~) (~~dry retention systems can be used towards open space~~): ~~65~~40%

H. Landscaping and bufferyard requirements meet or exceed the standards provided in Section 110-808 of the City's Land Development Code. A minimum of a 20 foot perimeter buffer shall be afforded around residential nodes of the project. Other areas of the project proposed for landscaping shall be depicted on the MDP. A Landscape Plan, illustrating the perimeter buffer yard widths, landscape materials, opacity of screening for adjacent land uses, internal landscape buffers between parcels, within parcels and any common areas shall be provided. Stormwater management facilities shall not be placed within bufferyards.

- I. Minimum lot size area (~~in acreage or square footage~~):**
1. Single Family Residential Lots: 5,750 sq. ft.
 2. Townhome Residential Lots: 2520 sq. ft.
- J. Minimum lot width (~~in feet~~):**
1. Single Family Residential Lots: 50 ft.
 2. Townhome Residential Lots: 24 ft.
- K. Minimum yard setbacks:**
1. Single Family Residential Lots
 - a) Front yard: 20 ft.
 - b) Side yard: 7.5 5 ft. (~~Minimum separation of eleven (11) feet required between eaves of adjacent buildings~~)
 - c) Street side yard: 10 ft.
 - d) Rear yard: ~~20~~ 10 ft.
 - e) Minimum floor area: 1,200 square ft.
 2. Townhome Residential Lots
 - a) Front yard: ~~25~~ 20 ft.
 - b) Side yard: N/A
 - c) Street side yard: N/A
 - d) Rear yard: ~~20~~ 10 ft.
 - e) Minimum floor area: 1,000 square ft. per unit
- L. Accessory Structures: Accessory structures shall be allowed for single family residential lots only; comply with all applicable City supplementary regulations as amended from time to time; located outside of any easements; and comply with the following setbacks:**
- a) Rear yard: 5 ft.
 - b) Side yard: 5 ft.
 - c) Street side yard: 10 ft.
- M. Maximum building height (~~in feet~~): 35 ft.**
- N. Cell tower: There is a parcel of land contained within the property proposed to be rezoned not owned/controlled by the Owner. This parcel, totaling approximately 0.50 acre in size, is developed with a 59.4 foot high monopole cell tower. The cell tower is not camouflaged. Residential lots within the subdivision shall be designed so no single family dwelling unit is located within the separation distance requirements of Chapter 82 of the City Land Development Code. However, no residential lots shall be plated/created within the design fall radius of the subject tower.**

- O. Minimum parking standards are per Sections. 110-828 and 110-829 of the City's Land Development Code. However, each townhome unit shall feature an enclosed two car garage.
- P. Minimum lighting standards per the City's Land Development Code shall be included on a separate Illumination Plan to be provided at the time of site plan and/or plat submittal.
- Q. Architectural controls and development on the Subject Property:
1. Single Family Residential Lots.
 - a) All structures shall complement one another and shall convey a sense of quality and permanence. Nothing contained herein shall require, nor preclude, a uniform architectural style or design aesthetic within the single family residential neighborhood. This shall permit, but not require, a diversification of architectural styles or designs available to homebuilders within the single family residential neighborhood. A homeowners association shall be formed to determine and enforce architectural expectations of the community. The City shall not be responsible for implementing architectural controls unless otherwise specified in this Agreement or other applicable City codes.
 - ~~b) Exterior walls facing public rights of way shall be constructed of finished materials such as stucco, natural brick or stone, finished concrete, wood or concrete fiberboard or other similar materials on all sides.~~
 - c) Utility boxes, Air conditioning condensers, pool pumps and similar mechanicals/apparatuses shall be screened from the public rights-of-way by architectural screening consistent with the structure or landscaping of sufficient density and maturity at planting to provide opaque screening.
 - ~~d) All controls and variations shall be defined by a Homeowners Association or Property Owners Association, as defined within this Agreement.~~
 2. Townhome Residential Lots.
 - a) All structures shall complement one another and shall convey a sense of quality and permanence. All structures shall follow a common architectural theme by harmoniously coordinating the general appearance of all buildings and accessory structures which may be visible

from a public right of way. This includes ~~an~~ the amenity center ~~if one is constructed~~. The purpose is to provide visual compatibility and functional continuity between the townhomes located within the Townhome Residential Lots. For purposes of this subparagraph, general appearance includes, but is not limited to, exterior wall finishes, construction materials, roof styles, slopes, architectural details and ornamentation.

- b) Exterior walls shall be constructed of finished materials such as stucco, natural brick or stone, finished concrete, wood or concrete fiberboard or other similar materials on all sides.
- c) The architectural elements of amenities, accessory structures and signage shall be consistent.
- d) All service areas and mechanical equipment visible from public rights-of-way (ground or roof) including but not limited to, air conditioning condensers, heating units, electric meters, irrigation pumps, ice machines, vending machines and other mechanicals, shall be screened using architectural features consistent with the structure, or landscaping of sufficient density and maturity at planting to provide opaque screening. Nothing contained herein shall be applied in violation of federal, state or local laws regarding the use of satellite television.
- e) All controls and variations shall be defined by a Homeowners Association ~~or Property Owners Association~~, as defined within this Agreement.

3. All utility distribution lines shall be located underground.

R. Utility provision and dedication: ~~The project will need to be served by central water, sewer and reuse water.~~ The Owner shall connect to the City of Deltona's central utility systems, ~~when available, or to Volusia County's central utility systems, where applicable,~~ and construct the required water, reuse and wastewater utility network at their sole cost and expense. Utility fees shall be paid to Deltona Water ~~or Volusia County, respectively,~~ before any building permit is issued. Central utility systems, both on site and off site, are to be designed, permitted, and constructed to ~~the respective service provider~~ City specifications and dedicated to the ~~respective service provider~~ City upon final inspection, clearance, and acceptance by the service provider.

S. Stormwater and environmental: ~~Per parcel stormwater systems or master~~ The ponds associated with the stormwater system shall be owned and maintained by an

established Homeowners Association ~~or Property Owners Association in private ownership~~ and shall not be dedicated to or become the responsibility of the City of Deltona. All environmental permitting, mitigation, and/or soil and erosion control for the property shall conform to all federal, state, and local permits/requirements, shall be the sole responsibility of the owner of the Homeowners Association or Property Owners Association, and shall be maintained by the Homeowners Association in good condition/standing with the applicable permitting authorities. Best Management Practices and conformance to National Pollutant Discharge Elimination System (NPDES) criteria are required. Finally, the stormwater system and related drainage will need to be sized/designed to manage flows from all impervious surfaces including a lot coverage allowance of 40%.

- T. Transportation, site access, and traffic devices: The Owner is responsible for all transportation improvements within the Subject Property and any off-site transportation requirements, as a result of the proposed development, for site function, that maintains or improves the level of service for area roadways, and ensures the public health, safety, and welfare for the community. All permits shall be obtained from appropriate permitting agencies prior to development and the City shall determine the appropriate level of service per the City Comprehensive Plan and current traffic counts. The MDP depicts an 80 foot wide access way from Doyle Road to the development nodes of the project. The 80 foot access road is depicted on the MDP intersecting with Doyle Rd. at right angles. However, to accommodate the right angle intersection the access way curves to the west off of the property owned/controlled by the Owner. The off-site property is triangular and accounts for approximately 25,000 square feet. Before a plat and/or site plan application is approved by the DRC for the subject property, the ownership/use of the aforementioned 25,000 square foot area needs to be secured through either fee simple acquisition or a legal instrument acceptable to the City Attorney. The project will generate 3,497 trips per day. These trips are projected to be distributed on Doyle Road via an 80 foot wide ingress/egress road. The following improvements will be required to accommodate access onto Doyle Road:

1) A 235-foot (storage and transition) eastbound left-turn lane from Doyle Rd. to the access road.

2) A 185-foot (storage and transition) westbound right-turn lane from Doyle Rd. to the access road.

3) Doyle Road westbound travel lane transition of no less than 405 feet east of the Doyle Rd./Vineland entrance road intersection and 300 feet west of the aforementioned intersection.

As per item 3 above the design of the turn lanes shall also include appropriate Doyle Road travel lane taper rates so travel lane functionality is not diminished. The above

improvements must be financed and constructed solely by the Owner and approved by the County before the first building permit is issued within the project. The above design requirements represent the minimum standard. The actual design requirements will be determined through the County Use Permit review process.

Sidewalks of a minimum of five feet of width shall be provided along both sides of all roadways within the project. The Vineland Preserve property abuts Osteen Elementary School. Therefore, the sidewalk network shall be connected to the school through a common boundary (in addition to a Doyle Rd. connection). The design and location of the pedestrian connection shall be determined between the Owner and the Volusia County School District.

The property abuts Collins Road along the northern terminus. The status of Collins Rd. involves both prescriptive and private elements. In addition, Collins Rd. does provide access to SR 415 but is not suitable to accommodate traffic associated with the Vineland Preserve project because of design limitations and lack of paving/stabilization. Vineland Preserve is designed with two outlets to Collins Rd. These outlets may be appropriate in the future as Collins Rd. may be improved. However, until Collins Rd. is improved to City specifications all access to Collins Rd. needs to be limited for emergency purposes only. Therefore, the access points on Collins Rd. need to be gated or otherwise impeded to only allow emergency access.

Finally, the medians associated with the entrance road shall be landscaped and irrigated with reuse water. The landscaping will include grass and other materials as deemed appropriate by the Owner and/or Homeowner's Association. The Homeowner's Association shall be responsible for the irrigation and maintenance of the landscape area.

The owner shall ensure full access to the cell tower site that is situated as an inholding within the Vineland Reserve project. The access configuration shall be addressed with the owner of the cell tower property and reflected on the plat as part of the plat process.

- U. Development Phases: The Subject Property may be developed at one time or in phases. If developed in phases, each phase must comply with and satisfy the terms and conditions provided for herein and the plat process as applicable per Chapter 106 of the City Land Development Code.
- V. Common Open Space: No less than 25% of the RPUD will be considered common open space. The common open space area shall be open for all residents for recreation and/or other leisure uses. Examples of common open space shall be the amenity center associated with the multi-family node and a trail system that extends through the natural vegetation retention area.

7. **Public Facilities/Land Dedication.** Facilities or tracts that either are or shall become public facilities/tracts that will serve the development and/or are on the Subject Property

are, as follows:

- A. All rights of way shall be designed, constructed and dedicated to the public as per City or as applicable County requirements. pending review and acceptance by the City.
- B. All on site and off site lift stations, water/sewer lines and related infrastructure shall be designed, constructed, and dedicated to the public as per City requirements. pending review and acceptance by the City.
- C. Collins Road as it may exist on the on property.
- D. ~~No additional facilities or lands are to be dedicated to the public.~~ Other dedications as deemed appropriate by the City which may arise during the plat and/or site plan review process.

8. **Development Permits/Fees.** The Owner is responsible for obtaining, permitting, and the payment of all fees for facilities and services ~~to ensure for~~ associated with the development and maintenance of the Subject Property. Any site permits shall be kept current with the respective permitting agency and shall ensure the protection of the public health, safety, and welfare of the City, surrounding community and the development. All impact fees are applicable and no impact fee credits shall be awarded through this Agreement.; ~~unless a cessation exists through a City moratorium that is Citywide.~~ Proportionate fair share site improvements shall not be used in lieu of City impact fees. Owner is responsible for County impact fees and at the discretion of the County may be required to engage in proportionate fair share activities or a similar type of arrangement.

9. **Site Plan/Plat Approval.** Exhibit "C", the Master Development Plan, is the ~~Preliminary Plan of the~~ graphic for this RPUD and this Agreement. The Master Development Plan shall not replace, supersede, or absolve the Owner from approvals for any site plan, preliminary plat, and/or final plat and their respective regulations. Where more detailed criteria for City required submittals exceed the criteria required for a Master Development Plan, the more detailed criteria applies.

10. **Indemnification.** The Owner shall indemnify and hold the City harmless from any and against all claims, demands, disputes, damages, costs, expenses, (to include attorneys' fees whether or not litigation is necessary and if necessary, both at trial and on appeal), incurred by the City as a result, directly or indirectly, of the use or development of the Subject Property, except those claims or liabilities caused by or arising from the negligence or intentional acts of the City, or its employees or agents. It is specifically understood that the City is not guaranteeing the appropriateness, efficiency, quality or legality of the use or development of the Subject Property, including but not limited to, drainage or water/sewer plans, fire safety, or quality of construction, whether or not inspected, approved, or permitted by the City.

11. **Compliance.** The Owner agrees that it, and its successors and assigns, will abide

by the provisions of this Agreement, the City's Comprehensive Plan and the City's Code of Ordinances, including but not limited to, the site plan regulations of the City as amended from time to time, which are incorporated herein by reference and such subsequent amendments hereto as may be applicable. Further, all required improvements, including landscaping, shall be continuously maintained by the Owner, or its successors and assigns, in accordance with the City's Code of Ordinances. The City may, without prejudice to any other legal or equitable right or remedy it may have, withhold permits, Certificates of Occupancy or plan/plat approvals to the Subject Property, should the Owner fail to comply with the terms of this Agreement. In the event of a conflict between this Development Agreement and the City's Land Development Code, the more restrictive regulations shall govern the development of the Subject Property.

12. **Obligations for Improvements.** Any surface improvement as described and required hereunder included, but not limited to such as signalization, walls, stormwater management facilities, medians, and utilities, or any other surface improvement shall be performed, prior to the issuance of the first Certificate of Occupancy on that portion of the Subject Property that the surface improvement(s) relates or is otherwise scheduled in this Agreement. Should the Owner fail to undertake and complete its obligations as described in this Agreement and to the City's specifications, then the City shall give the Owner thirty (30) days written notice to commence and ninety (90) days to complete said required obligation at the sole expense of the Owner. If the Owner fails to complete the obligations within the ninety (90) day period, then the City, without further notice to the Owner and their successors and assigns in interest, may but shall not be required to, perform such obligations at the expense of the Owner or their successors and assigns in interest, without prejudice to any other rights or remedies the City may have under this Agreement. Further, the City is hereby authorized to immediately recover the actual and verified cost of completing the obligations required under this Agreement and any legal fees from the Owner in an action at law for damages, as well as record a lien against the Subject Property in that amount. The lien of such assessments shall be superior to all others, and all existing lienholders and mortgagees, by their execution of the subordination or joinder documents, agree to subordinate their liens or mortgages to the City's said liens or assessments. Notice to the Owner and its successors and assigns in interest shall be deemed to have been given upon the mailing of notice as provided in paragraph (19) of this Agreement.

13. **Concurrency and Vested Rights.** The Owner acknowledges and agrees that prior to the issuance of any development orders for the Property, the Owner must have received and be in the possession of a valid unexpired certificate of capacity/concurrency management system approval consistent with the City's Land Development Code. The capacity certificate/approval verifies the availability of infrastructure and service capacity sufficient to permit the proposed development of the Subject Property without causing a reduction in the levels of service adopted in the City's Comprehensive Plan. The certificate of capacity/approval shall be effective for a term, as defined in the City's Code of Ordinances. Neither this Agreement nor the approved Master Development Plan shall create or result in a vested right or rights to develop the Subject Property, as cited in Section 86-34 of the City's Land Development

Code.

14. **Environmental and Tree Preservation.** The Owner is responsible to obtain all site related permits and approval prior to any development activity on or for the Subject Property. This may involve mitigation for habitat of threatened or endangered flora and fauna or for species identified for proportion (i.e. tree preservation). This Agreement does not vest or exempt the Owner from any permitting and mitigation obligations needed to develop a Subject Property. The property contains gopher tortoise habitat. The site needs to be surveyed and if deemed necessary, tortoises relocated. Any relocation of tortoises needs to be consistent with applicable permitting agencies.

15. **Homeowners Association or Property Owners Association.** The charter and by-laws of ~~any~~ a Homeowners Association (“HOA”) ~~or Property Owners Association (“POA”)~~ for the Subject Property and any deed restrictions related thereto shall be furnished to the City for approval by the City Attorney prior to the recording thereof in the Public Records of Volusia County, Florida. Such recording shall take place before a Certificate of Occupancy is issued for the first development project on land covered by this Agreement. ~~The HOA or POA~~ shall at a minimum be responsible for maintaining the common open space, stormwater ponds, amenities, any common utility systems, such as for irrigation, landscaping and site lighting, and project signage and implementing the architectural and aesthetic requirements. The Owner shall be responsible for establishing the HOA ~~or POA~~ and recording said information in the Public Records of Volusia County, Florida. The City is not responsible for the enforcement of any agreements or deed restrictions entered into between property owners or occupiers of the Subject Property. If maintenance for the Subject Property is not maintained following issuance of a Certificate of Occupancy, the City ~~has~~ will engage Code Enforcement services.

16. **Enforcement and Venue.** Both parties may seek specific performance of this Agreement and/or bring an action for damages in a court within Volusia County, Florida, if this Agreement is breached by either party. In the event that enforcement of this Agreement by the City becomes necessary, and the City is successful in such enforcement, the Owner shall be responsible for the payment of all of the City’s costs and expenses, including attorney fees, whether or not litigation is necessary and, if necessary, both at trial and on appeal. Such costs, expenses and fees shall also be a lien upon the Subject Property superior to all others. Should this Agreement require the payment of any monies to the City, the recording of this Agreement shall constitute a lien upon the Subject Property for said monies, until said are paid, in addition to such other obligations as this Agreement may impose upon the Subject Property and the Owner. Interest on unpaid overdue sums shall accrue at the rate of the lesser of eighteen percent (18%) compounded annually or at the maximum rate allowed by law.

17. **Utility Easements.** For any easement not established on a plat for the Subject Property, the Owner shall provide to the City such easements and other legal documentation, in form mutually acceptable to the City Attorney and the Owner, as the City may deem reasonably

necessary or appropriate for the installation and maintenance of the utility and other services, including but not limited to, sanitary sewer, potable water, and reclaimed water services, electric, cable, gas, fire protection and telecommunications.

18. **Periodic Review.** The City reserves the right to review the Subject Property subject in relation to this Agreement periodically to determine if there has been demonstrated good faith compliance with the terms of this Agreement. If the City finds that on the basis of substantial competent evidence that there has been a failure to comply with the terms of this Agreement, the City may not issue development orders or permits until compliance with this Agreement has been established.

19. **Notices.** Where notice is herein required to be given, it shall be by certified mail return receipt requested, hand delivery or nationally recognized courier, such as Federal Express or UPS. E-mail delivery of documents shall not replace or be in lieu of the aforementioned process. Said notice shall be sent to the following, as applicable:

OWNER REPRESENTATIVES:

James Skinner
Lake Disston Lands LLC
230 North Woodland Boulevard, Suite 304
DeLand, Florida 32720

WITH A COPY TO:

Michael J. Woods, Esquire
CobbCole
351 East New York Avenue; Suite 200
DeLand, Florida 32724

CITY'S REPRESENTATIVES:

City Manager
City of Deltona
2345 Providence Boulevard
Deltona, Florida 32725

WITH A COPY TO:

Director, Planning & Development Services
City of Deltona
2345 Providence Boulevard
DeLand, Florida 32725

Should any party identified above change, it shall be said party's obligation to notify the remaining parties of the change in a fashion as is required for notices herein. It shall be the Owner/Developer's or Developer obligation to identify its lender(s) to all parties in a fashion as is required for notices herein.

20. **Compliance with the Law.** The failure of this Agreement to address a particular permit, condition, term, or restriction shall not relieve the Owner of the Subject Property from the necessity of complying with the law governing said permitting requirements, conditions, terms, or restrictions.

21. **Captions.** The captions used herein are for convenience only and shall not be relied upon in construing this Agreement.

22. **Binding Effect.** This Agreement shall run with the Subject Property, shall be binding upon and inure to the benefit of the Owner and its successors and assigns in interest, and the City and their successor and assigns in interest. This Agreement shall become effective upon its execution and recordation with the Public Records of Volusia County, Florida. This Agreement does not, and is not intended to, prevent or impede the City from exercising its legislative authority as the same may affect the Subject Property.

23. **Subsequently Enacted State or Federal Law.** If either state or federal law is enacted after the effective date of this Agreement that is applicable to and precludes the parties' compliance with the terms of this Agreement, this Agreement and correlating zoning amendment shall be modified or revoked, as is necessary, to comply with the relevant state or federal law.

24. **Severability.** If any part of this Development Agreement is found invalid or unenforceable in any court, such invalidity or unenforceability shall not affect the other parts of this Development Agreement, if the rights and obligations of the parties contained herein are not materially prejudiced and if the intentions of the parties can be affected. To that end, this Development Agreement is declared severable.

25. **Recordation of Agreement.** The parties hereto agree that an executed original of this Agreement shall be recorded by the City, at the Developer's expense, in the Public Records of Volusia County, Florida.

26. **Time of the Essence.** Time is hereby declared of the essence to the lawful performance of the duties and obligations contained in this Agreement. The Owner shall execute this Agreement within ten (10) business days of City Commission adoption of Ordinance No. ____; and agrees to pay the cost of recording this document in the Public Records of Volusia County, Florida. Failure to execute this Agreement within ten (10) business days of this ordinance adoption may result in the City not issuing development orders or permits until execution and recordation of this Agreement has occurred.

27. **Agreement; Amendment.** This Agreement constitutes the entire agreement between the parties, and supersedes all previous discussions, understandings and agreements, with respect to the subject matter hereof; provided, however, that it is agreed that this Agreement is supplemental to the City's Comprehensive Plan and does not in any way rescind or modify any provisions of the City's Comprehensive Plan. Amendments to and waivers of the provisions of this Agreement shall be made by the parties only in writing by formal amendment.

28. **Effective Date.** The Effective Date of this Agreement shall be the day this Agreement is recorded in the Public Records of Volusia County, Florida.

[REMAINDER OF PAGE INTENTIONALLY BLANK]

IN WITNESS WHEREOF, the Owner, and the City have executed this Agreement.

OWNER

Lake Disston Lands LLC

By:

Signature of Witness # 1

Signature

Print or type name

Print or type name

As:

Signature of Witness #2

Print or type

ATTEST:

Print or type name

Signature

Print or Type Name

As:

Mailing Address: _____

STATE OF FLORIDA
COUNTY OF _____

The foregoing instrument was acknowledged before me this _____ day of _____, 201__, by _____, and _____, of _____, who is/are personally known to me or who has/have produced _____ as identification and who did not (did) take an oath.

Signature of Notary

(NOTARY SEAL)

Print or type name

CITY OF DELTONA:

By:

Date:

ATTEST:

Date:

Mailing Address:
City of Deltona
2345 Providence Boulevard
Deltona, Florida 32725

STATE OF FLORIDA
COUNTY OF _____

The foregoing instrument was acknowledged before me this ____ day of _____, 201__, by _____, and _____, who are personally known to me and acknowledge executing the same freely and voluntarily under authority vested in them by the City of Deltona.

Signature of Notary

(NOTARY SEAL)

Print or type name

Approved as to form and legality for use and
reliance by the City of Deltona, Florida

Gretchen R. H. Vose, Esq.
City Attorney

EXHIBIT B
LEGAL DESCRIPTION

OR 6801, PAGE 3579

A portion of the North 1/2 of Section 12, Township 19 South, Range 31 East, Volusia County, Florida, more particularly described as follows:

Commence at the North 1/4 corner of said Section 12; thence South 00°12'06" East, 35.00 feet to the point of beginning; thence North 89°59'16" East, 1333.40 feet; thence South 00°24'06" East, 1285.85 feet; thence North 89°57'56" East, 1337.92 feet, to a point on the East line of the Northeast 1/4 of said Section 12; thence South 00°35'15" East, along said line 660.18 feet; thence South 89°57'15" West, 1340.18 feet; thence North 00°32'09" West, 480.44 feet; thence South 89°59'40" West, 178.79 feet; thence North 00°23'29" West, 180.04 feet; thence South 89°59'36" West, 1440.94 feet to a point on the East line of the West 31.0 acres of the Northeast 1/4 of the Northwest 1/4 of said Section 12; thence North 00°26'19" West, along said line, 1285.77 feet; thence South 89°59'21" East, 288.37 feet to the Point of Beginning Said lands lying in Volusia County, Florida.

Less

A portion of the North 1/2 of Section 12, Township 19 South, Range 31 East, Volusia County, Florida more particularly described as follows:

Commence at the Northeast corner of said Section 12; thence South 89°59'26" West, along the North line of the Northeast 1/4 of said Section 12, a distance of 2622.03 feet; thence South 00°12'15" East, 1004.55 feet; thence South 89°47'45" West, 104.82 feet to the Point of Beginning; thence South 00°12'15" East, a distance of 220.00 feet; thence South 89°47'45" West, 200.00 feet; thence North 00°12'15" West, 220.00 feet; thence North 89°47'45" East, 200.00 feet to the Point of Beginning. Said lands lying in Volusia County, Florida.

TOGETHER WITH:

A portion of Section 12, Township 19 South, Range 31 East, Volusia County, Florida, more particularly described as follows:

Commence at the Northwest corner of the Southwest one-quarter of the Northeast one-quarter of said Section 12; thence South 89°47'05" East along the North line of said Southwest one-quarter, 1257.97 FEET; thence South 00°03'57" East, 180.00 feet to the point of beginning; thence South 89°47'05" East, 80.00 feet, to a point on the West line of the Southeast one-quarter, of the Northeast one-quarter of said Section 12; thence South 00°03'57" East, along said line, 1532.84 feet to a point on the North right-of-way Line of Doyle Road; thence North 71°58'36" West, along said line, 84.16 feet; thence North 00°03'57" West, 1507.10 feet to the point of beginning.

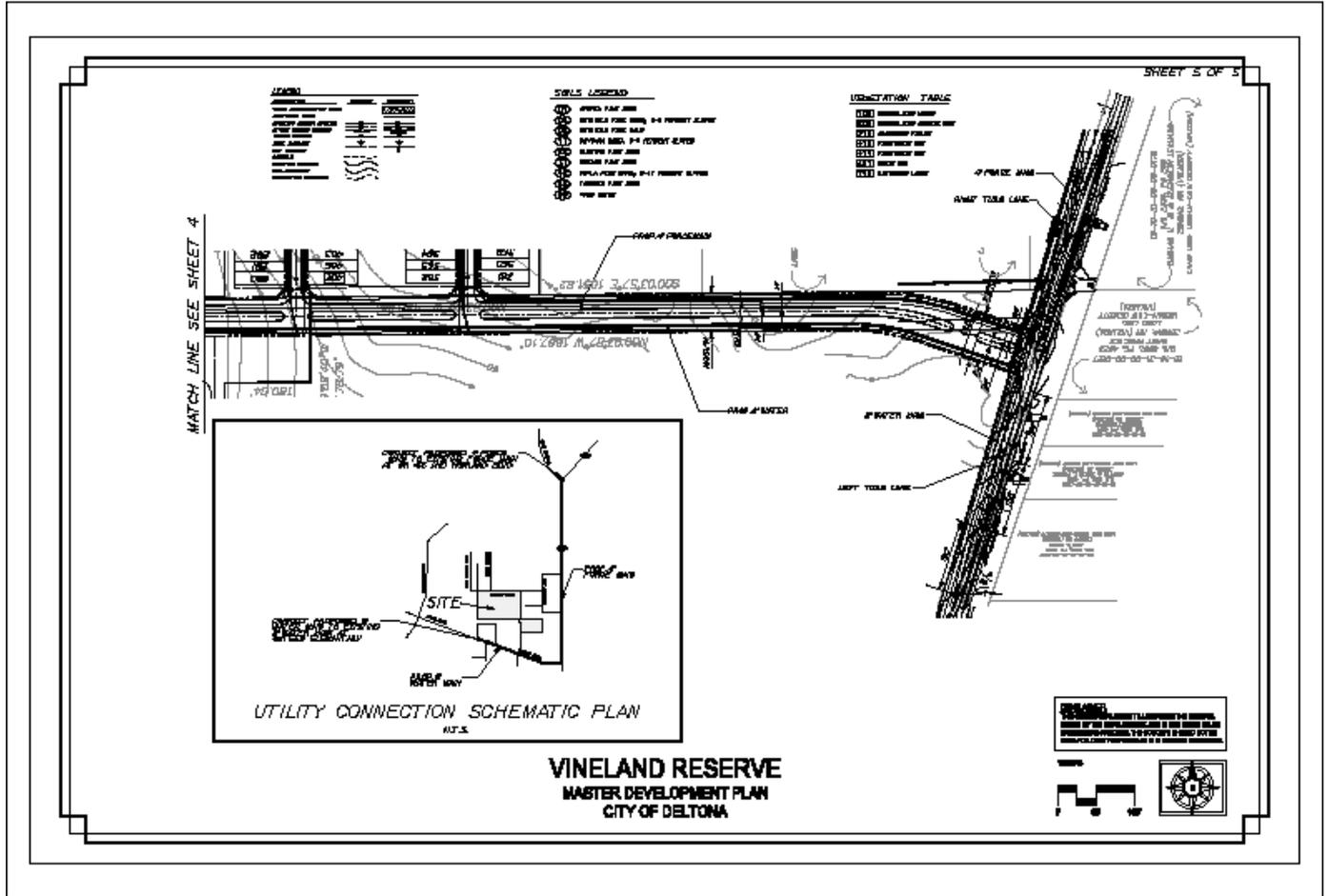
AND TOGETHER WITH

OR 6969, PAGE 3583

THE WEST 31 ACRES OF THE NORTHEAST ONE-QUARTER OF THE NORTHWEST ONE-QUARTER OF SECTION 12, TOWNSHIP 19 SOUTH, RANGE 31 EAST, VOLUSIA COUNTY, FLORIDA MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCE AT THE NORTH ONE-QUARTER CORNER OF SAID SECTION 12;
THENCE NORTH 89°59'21" WEST, ALONG THE NORTH LINE OF THE NORTHWEST ONE-QUARTER OF SAID SECTION 12, A DISTANCE OF 288.63 FEET; THENCE SOUTH 00°26'19" WEST, 35.00 FEET TO THE POINT OF BEGINNING; THENCE SOUTH 00°26'19" EAST, A DISTANCE OF 1283.10 FEET; THENCE SOUTH 00°26'19" EAST, 2.68 FEET; THENCE SOUTH 89°59'40" WEST, 1045.57 FEET; THENCE NORTH 00°26'19" WEST, 1321.08 FEET; THENCE SOUTH 89°59'21" EAST, 167.56 FEET; THENCE SOUTH 00°00'39" WEST, 35.00 FEET; THENCE SOUTH 89°59'21" EAST, 878.28 FEET TO THE POINT OF BEGINNING SAID LANDS LYING IN VOLUSIA COUNTY, FLORIDA.

EXHIBIT C MASTER DEVELOPMENT PLAN



TRAFFIC IMPACT STUDY

For
Vineland Reserve

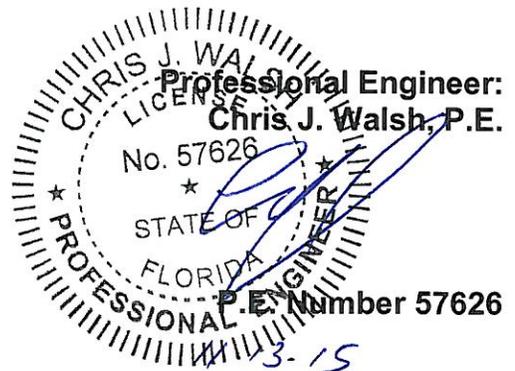
Deltona, Florida

Prepared for:

Pell Properties



Traffic Engineering Data Solutions, Inc.
80 Spring Vista Drive
DeBary, Florida 32713
November 2015



INTRODUCTION

Traffic Engineering Data Solutions, Inc. (TEDS) has been retained to conduct a traffic impact analysis for the Vineland Reserve development located on the north side of Doyle Road approximately 0.46 miles west of State Road 415 in the City of Deltona, Florida (see **Figure 1**). The proposed development will include 279 single-family residential units and 128 townhomes. A preliminary site plan of the proposed development is included in the **Appendix**. The proposed development is scheduled for build out in 2017.

This study, which evaluates the overall impact of the development on the adjacent roadway network, was prepared for the City of Deltona's transportation concurrency requirements. This study was conducted in accordance with the methodology as provided in the **Appendix**.

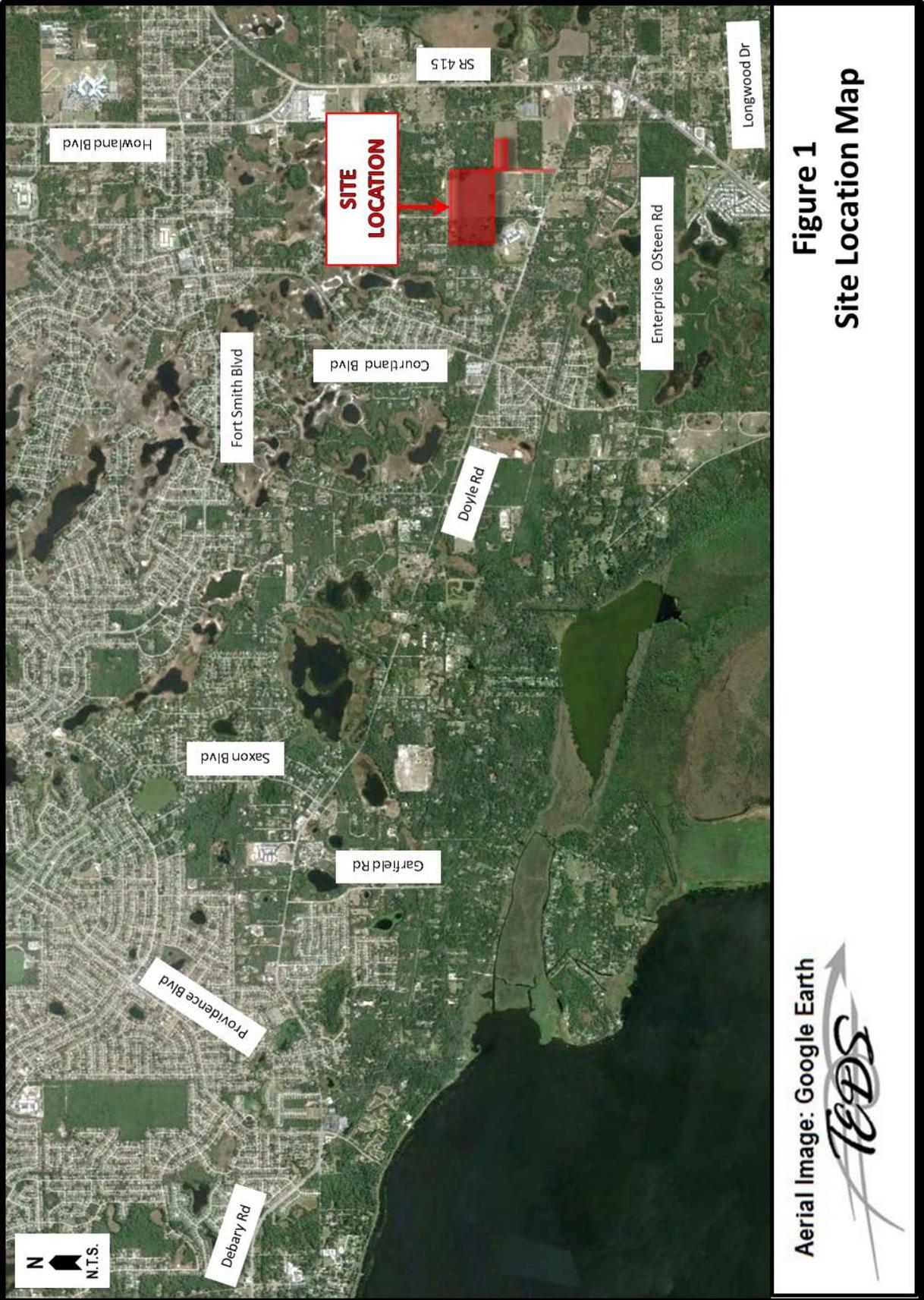
PROJECT ACCESS

Access to the proposed development is proposed via one full access roadway off of Doyle Road approximately 1.06 miles east of Courtland Boulevard and 0.46 miles west of State Road 415.

STUDY AREA

Because the proposed development is projected to generate more than 300 two-way peak hour external trips, the study area was determined based upon a five-percent level of significance as consistent with the River to Sea TPO Transportation Impact Analysis (TIA) Guidelines. Based on the methodology, Doyle Road, from Providence Boulevard to State Road 415, was determined to be the study roadway. The study intersections include the following:

- Doyle Road at Providence Boulevard
- Doyle Road at Saxon Boulevard
- Doyle Road at Courtland Boulevard
- Doyle Road at Proposed Development Roadway
- Doyle Road at State Road 415



EXISTING CONDITIONS

Existing Volumes

For purposes of this study, PM peak-period turning movement counts, from 4:00 PM to 6:00 PM, were conducted at the study intersections. **Figure 2** summarizes the existing PM peak-hour turning movement volumes at the study intersections. Printouts of the traffic counts are provided in the **Appendix**.

Roadway Segments

The PM peak-hour two-way volumes on the roadway segments were calculated by obtaining existing daily volumes (2014) from Volusia County and factoring the volumes to two-way PM peak-hour volumes by applying a K-factor of 0.091. The resulting volumes were then compared against the generalized service volume for each study roadway segment. The generalized peak-hour two-way service volume for each roadway segment was obtained from Volusia County's 2014 Average Annual Daily Traffic & Historical Counts based on the adopted level of service standards from the City of Deltona's Comprehensive Plan. **Table 1** below shows the adopted level of service and generalized service volume under the adopted level of service for each study roadway segment. As shown in **Table 1**, the existing PM peak-hour two-way volumes for all of the study roadway segments are below the generalized service volume, thereby indicating that all roadway segments currently have acceptable operating conditions.

Table 1
Existing Roadway Segment Operating Conditions (PM Peak Hour)

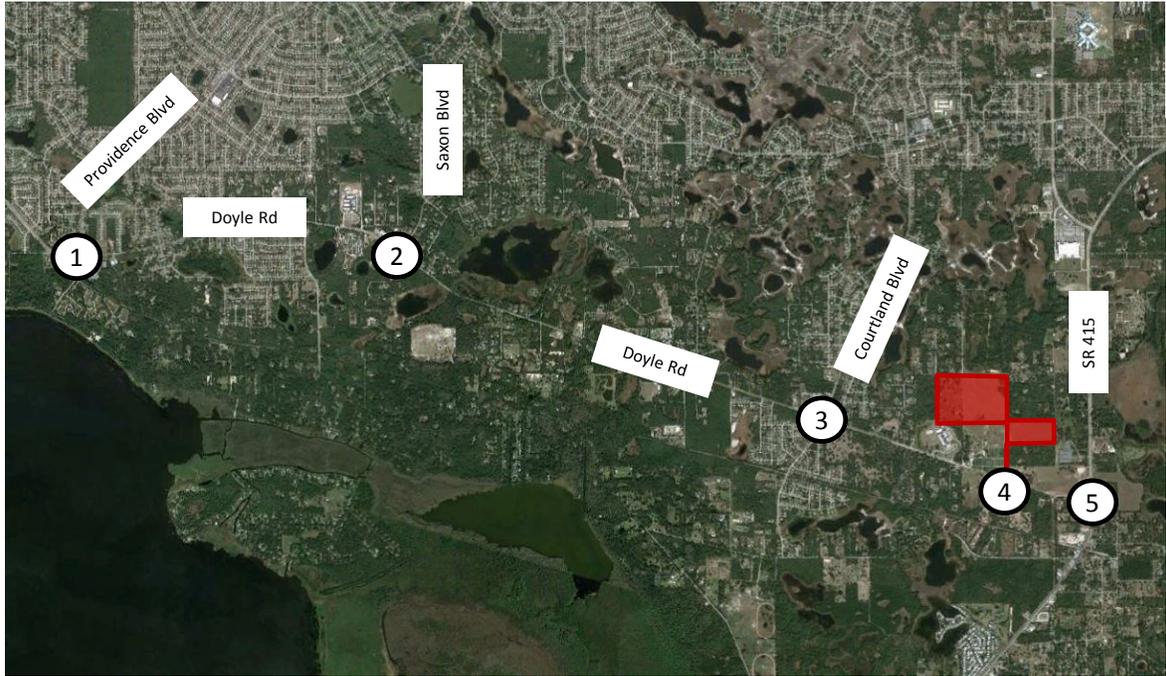
Roadway Segment	Existing Number of Lanes	Adopted Level of Service Standard ¹	Pk-Hr 2-Way Generalized Service Volume ²	Existing AADT	Existing 2-Way PM Peak-Hour Volumes ³	Year of Count	Existing Volume Exceeds Service Volume?
Doyle Road							
Providence Blvd to Garfield Road	2	E	1,230	11,950	1,087	2014	No
Garfield Road to Saxon Blvd	2	E	1,230	9,240	841	2014	No
Saxon Blvd to Courtland Blvd	2	E	1,230	7,670	698	2014	No
Courtland Blvd to Project Access Dwy	2	E	1,230	5,450	496	2014	No
Project Access Dwy to SR 415	2	E	1,230	5,450	496	2014	No

1 - Adopted levels of service obtained from City of Deltona's Comprehensive Plan

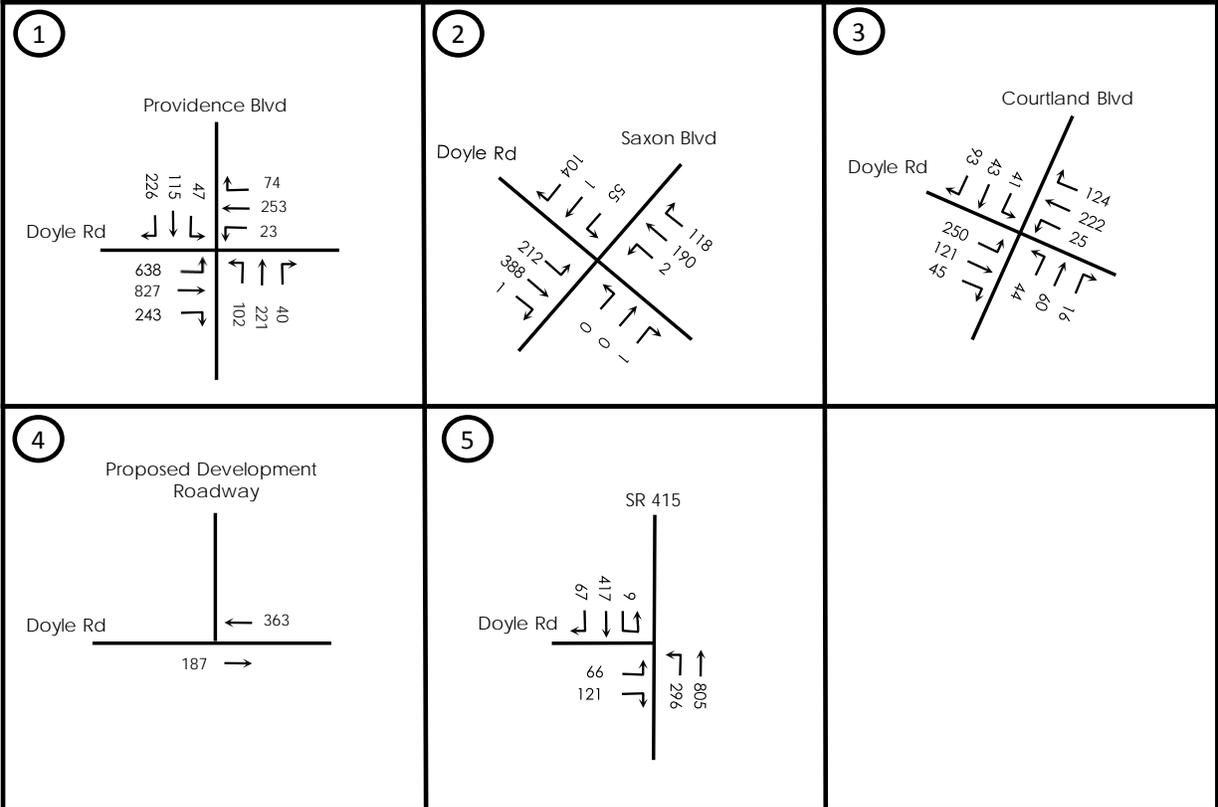
2 - Generalized service volumes for the adopted level of service were obtained from Volusia County.

3 - Existing PM peak-hour 2-way volumes calculated by applying a k-factor of 0.091 to the existing AADT volumes

Figure 2: Existing PM Peak-Hour Turning Movements



Aerial Image: Google Earth



Intersections

The PM peak-hour existing operating conditions of the study intersections were evaluated using the Highway Capacity Software (HCS) 2010 which utilizes analysis methodologies contained in the 2010 Highway Capacity Manual. The existing PM peak-hour turning movement volumes, existing roadway geometry, and existing signal timings were utilized in the analyses. Based on the HCS analyses, the existing overall level of service (LOS) and delay for the study intersections are as follows:

<u>Intersection</u>	<u>Overall LOS</u>	<u>Overall Delay (sec/veh)</u>
Doyle Road/Providence Boulevard	E	71.5
Doyle Road/Saxon Boulevard	B	10.1
Doyle Road/Courtland Boulevard	B	14.1
Doyle Road/State Road 415	B	16.3

HCS printouts are provided in the **Appendix**.

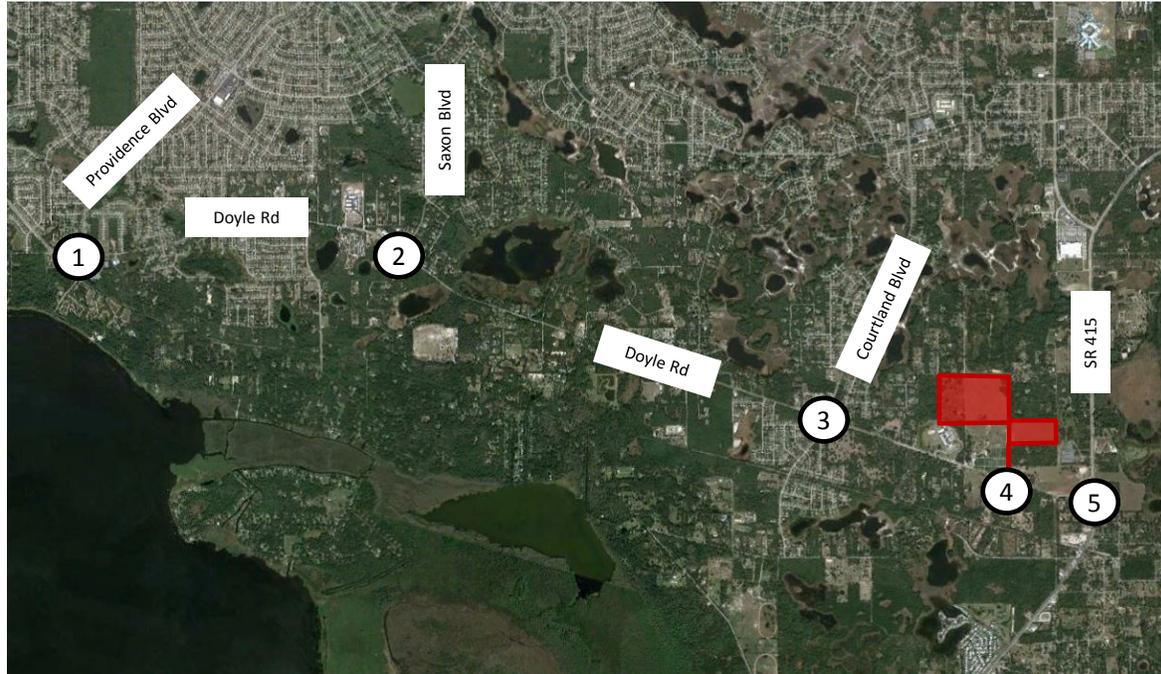
FUTURE BACKGROUND TRAFFIC

Future background traffic is the non-project-related traffic projected to utilize the study roadways and intersections. For the purposes of this analysis, the future background traffic was calculated by applying an annual growth rate to existing PM peak-hour volumes to factor the volumes up to year 2017, the year the development is proposed to be open. The annual growth rates were based on historical annual growth rates calculated from historical traffic data as obtained from Volusia County. Upon using the Traffic Trends Analysis Tool, it was concluded that the historical annual growth rates for Doyle Road, from Providence Boulevard to State Road 415, for the 2004 to 2014 analysis period have been negative. For the purposes of providing a conservative analysis, a one-percent annual growth rate was applied to the existing volumes to factor them to year 2017 future background PM peak-hour volumes. **Figure 3** shows the future background PM peak-hour turning movements at the study intersections while **Table 2** shows the future background PM peak-hour two-way volumes on the study roadway segments. Traffic Trends Analysis Tool printouts are also provided in the **Appendix**.

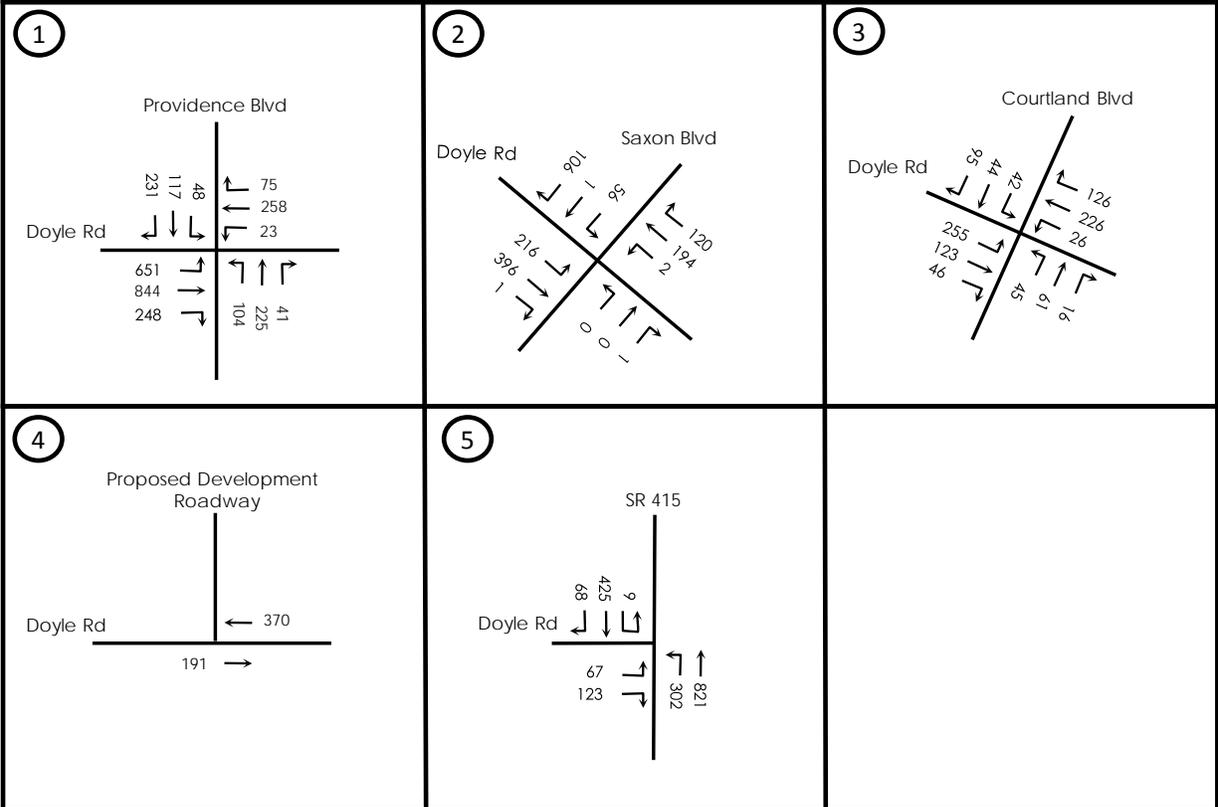
Table 2
Future Background Volumes for Roadway Segments (PM Peak Hour)

Roadway Segment	Existing 2-Way PM Peak-Hour Volumes	Year of Count	Annual Growth Rate	Future Background PM Pk-Hr 2-Way Volume
Doyle Road				
Providence Blvd to Garfield Road	1,087	2014	1.0%	1,120
Garfield Road to Saxon Blvd	841	2014	1.0%	866
Saxon Blvd to Courtland Blvd	698	2014	1.0%	719
Courtland Blvd to Project Access Dwy	496	2014	1.0%	511
Project Access Dwy to SR 415	496	2014	1.0%	511

Figure 3: Future Background PM Peak-Hour Turning Movements



Aerial Image: Google Earth



TRIP GENERATION

The number of vehicle trips that will originate from or are destined to a development is called trip generation and is dependent upon the type and size of the development. The total daily and PM peak-hour trip generation potential for the proposed development was determined based on trip generation equations and rates provided in the Institute of Transportation Engineer's (ITE) Informational Report, *Trip Generation, 9th Edition*. For the proposed development, ITE Land Use Code 210 (Single-Family Detached Housing) and ITE Land Use Code 230 (Residential Condominium/Townhouse) were used. As summarized in **Table 3**, the proposed development is projected to generate 3,497 total daily trips and 338 total PM peak-hour trips (216 in, 122 out).

Table 3
Trip Generation Projection for Proposed Development

Land Use	Intensity	Units	Daily			PM Peak		
			In	Out	Total	In	Out	Total
Single-Family Detached Housing	279	Dwelling Units	1350	1,349	2,699	167	98	265
Residential Condominium/Townhouse	128	Dwelling Units	399	398	797	49	25	74
Net New External Trips			1,749	1,748	3,497	216	122	339

Single-Family Detached Housing (ITE 9th Edition - Land Use Code 210)

Daily	$LN (T) = 0.92 \times LN (\# \text{ of DU}) + 2.72$	50% In	50% Out
PM Peak Hour	$LN (T) = 0.90 \times LN (\# \text{ of DU}) + 0.51$	63% In	37% Out

Residential Condominium/Townhouse (ITE 9th Edition - Land Use Code 230)

Daily	$LN (T) = 0.87 \times LN (\# \text{ of DU}) + 2.46$	50% In	50% Out
PM Peak Hour	$LN (T) = 0.82 \times LN (\# \text{ of DU}) + 0.32$	67% In	33% Out

TRIP DISTRIBUTION

The trip distribution pattern defines the primary corridors that will be traveled by the traffic generated by the project. The approved trip distribution, as included in the approved methodology, was developed based on applying engineering judgment and using local knowledge of typical travel patterns for residents within the area. The trip distribution is shown in **Figure 4**.

TRIP ASSIGNMENT

The new external PM peak-hour project trips were assigned to the study roadways and intersections based on the trip distribution. **Figure 5** shows the PM peak-hour new external trips, assigned to the study intersections.

The project trips were then added to the future background traffic volumes to arrive at the total future PM peak-hour volumes for both the roadway segments and intersections. **Figure 6** shows the total (year 2017) PM peak-hour turning movement projections at the study intersections at build out of the development. Turning movement worksheets are provided in the **Appendix**. **Table 4** summarizes the total PM peak-hour two-way volumes in year 2017 on the roadway segments at build out of the development.

Table 4
Year 2017 Roadway Segment Volumes and Operating Conditions (PM Peak Hour Two-Way)

Roadway Segment	# of Lanes	Future Background PM Pk-Hr 2-Way Volume	Percent Assignment	2-Way Pk-Hr Project Trips	Future Total 2-Way PM Pk-Hr Volume	Adopted Level of Service Standard	Pk-Hr 2-Way Generalized Service Volume	Future Total Volume Exceeds Service Volume?
Doyle Road								
Providence Blvd to Garfield Road	2	1,120	25.0%	85	1,205	E	1,230	No
Garfield Road to Saxon Blvd	2	866	25.0%	85	951	E	1,230	No
Saxon Blvd to Courtland Blvd	2	719	30.0%	101	820	E	1,230	No
Courtland Blvd to Project Access Dwy	2	511	35.0%	118	629	E	1,230	No
Project Access Dwy to SR 415	2	511	65.0%	220	731	E	1,230	No

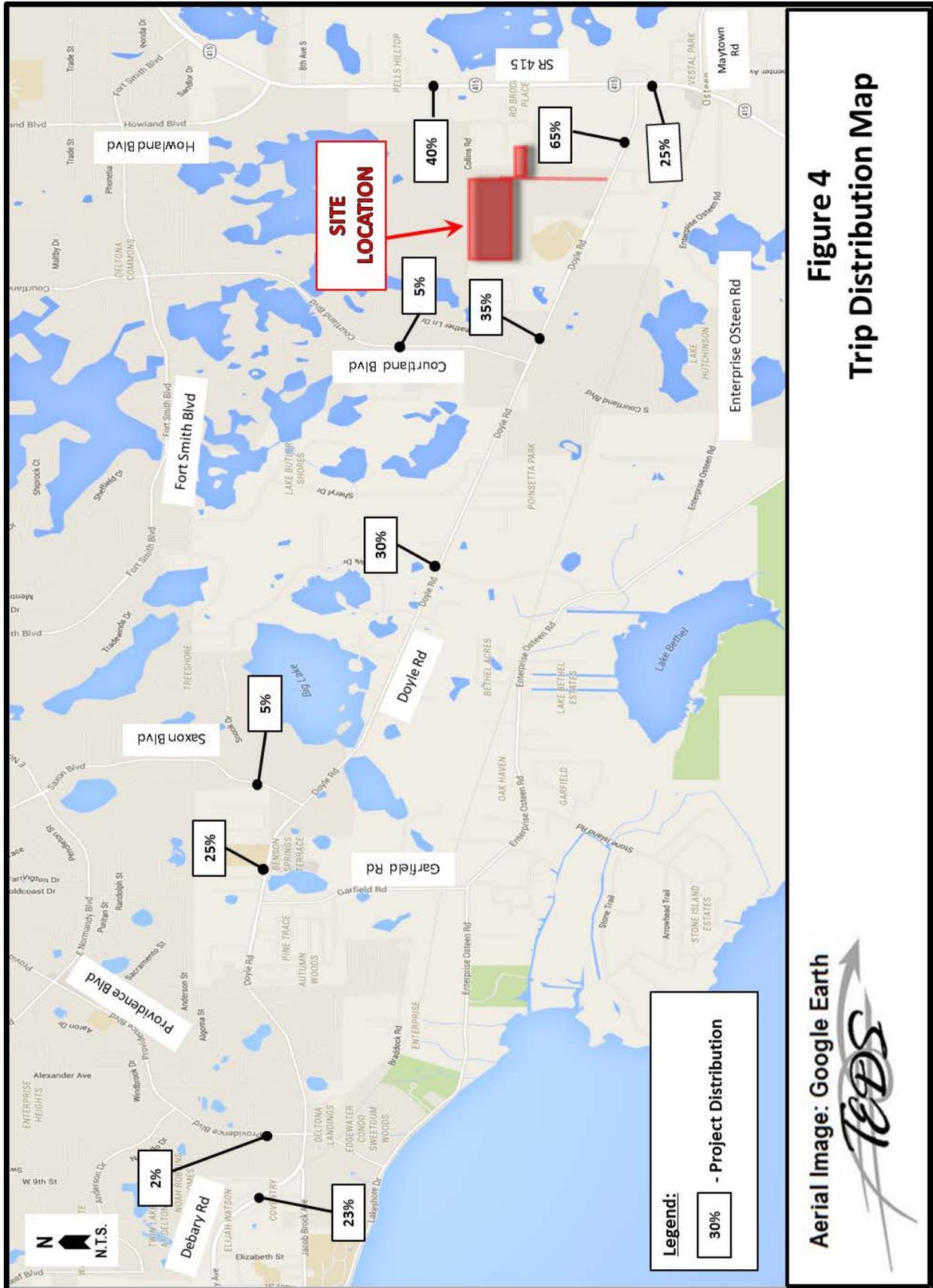


Figure 5: Future New External PM Peak-Hour Turning Movements



Aerial Image: Google Earth

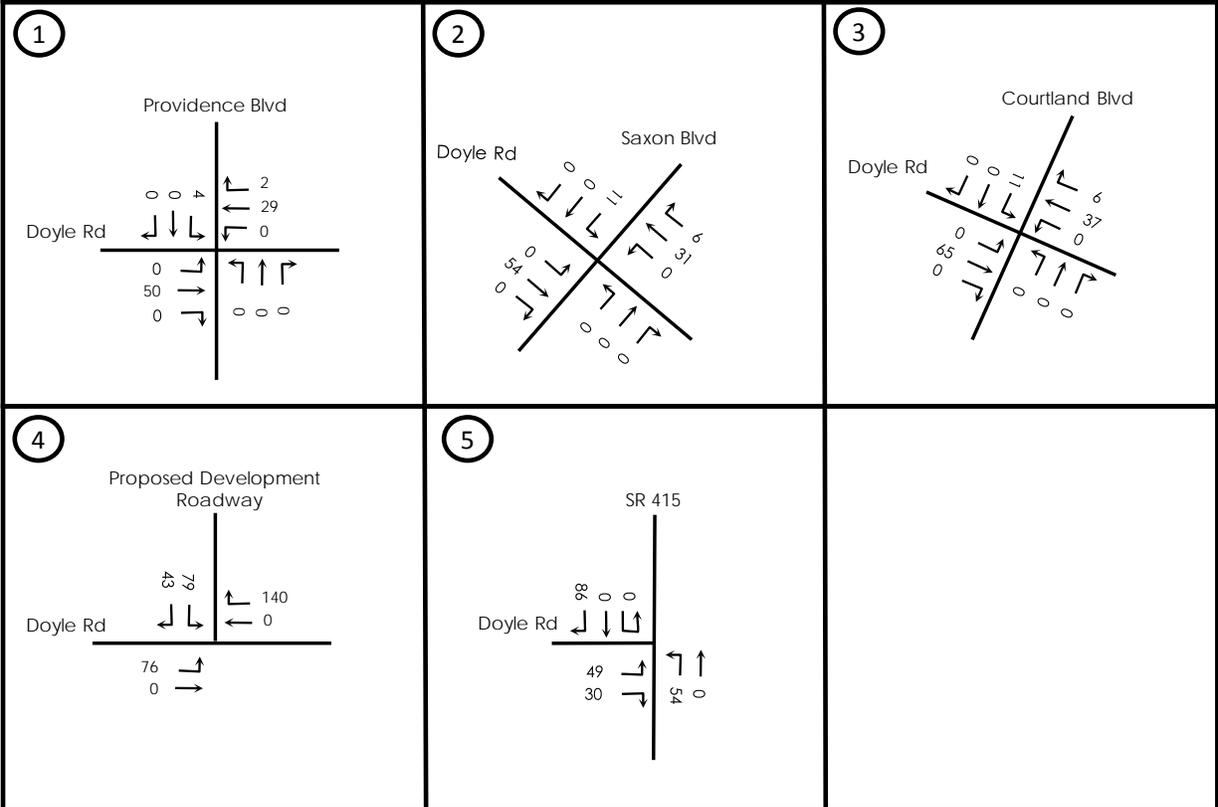
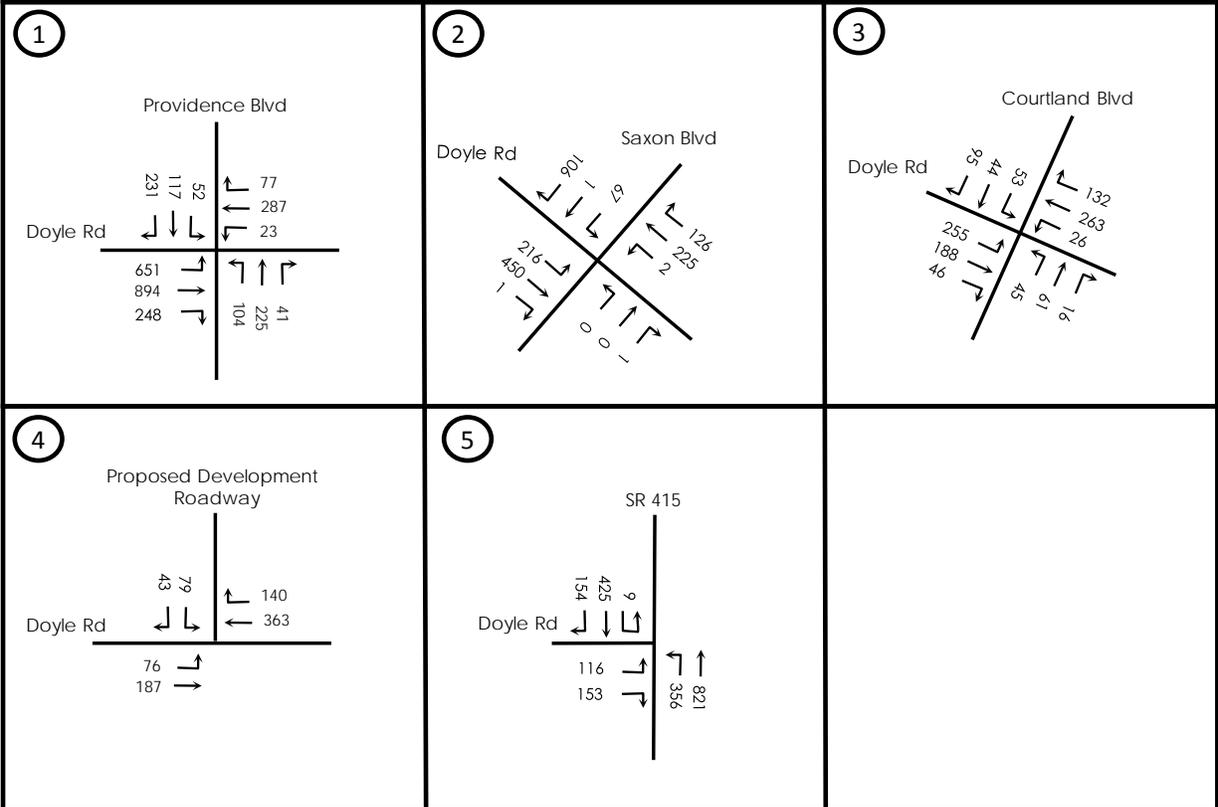


Figure 6: Future Total PM Peak-Hour Turning Movements



Aerial Image: Google Earth



FUTURE CONDITIONS ANALYSIS

Roadway Segments

The PM peak-hour operating conditions of the study roadway segments were analyzed by comparing total projected PM peak-hour two-way segment volumes to each roadway segment's generalized service volume. As summarized in **Table 4**, the projected volumes on all study roadway segments are below the generalized service volumes. Therefore, all study roadway segments are projected to have acceptable operating conditions in year 2017 at build out of the proposed Vineland Reserve development.

Intersections

The PM peak-hour operating conditions for the study intersections along Doyle Road were analyzed at build out of the proposed development in year 2017 using HCS 2010 and the projected turning movements. Based on the HCS analyses, the projected overall level of service (LOS) and delay for the signalized study intersections, at buildout of the proposed development, are as follows:

Intersection	Overall LOS	Overall Delay (sec/veh)
Doyle Road/Providence Boulevard	F	133.8
Doyle Road/Saxon Boulevard	B	10.4
Doyle Road/Courtland Boulevard	B	14.3
Doyle Road/State Road 415	B	18.7

It should be noted that with optimized signal timings (increasing the Max green for the eastbound/westbound left-turn movements to 25 seconds and increasing the eastbound/westbound through movements to 65 seconds) the Doyle Road/Providence Boulevard intersection is projected to operate acceptably at LOS D (overall delay of 44.9 seconds/vehicle). The HCS printouts are provided in the **Appendix**.

Additionally, the intersection of the development road with Doyle Road was analyzed under two-way STOP control using HCS. Based on the HCS analyses, the eastbound left-turn movement is projected to operate acceptably at LOS A (average delay of 8.7 seconds/vehicle). The southbound approach is also projected to operate acceptably at LOS C (average delay of 15.5 seconds/vehicle).

It should be noted that the eastbound left-turn volume is projected to exceed 25 vehicles in the peak hour and thus a left-turn lane on Doyle Road will be required per Volusia County requirements. The 95th percentile queue is less than one vehicle, therefore, a turn-lane length of 205 feet is recommended (based on a 50-foot queue and 155 feet of deceleration per FDOT's 2016 Design Standards, Index 301).

Additionally, the westbound right-turn volume exceeds 100 vehicles per hour, thus a westbound right-turn lane on Doyle Road will also be needed per Volusia County requirements. Because the movement is free flow, there is no projected queue. Therefore, a turn-lane length of 155 feet is recommended (based on 155 feet of deceleration per FDOT's 2016 Design Standards, Index 301).

ALTERNATIVE MODE ANALYSIS

Per the River to Sea TPO TIA Guidelines, an evaluation relating to transit, pedestrian, and bicycle facilities is provided below.

Transit – Votran currently has no routes that pass by the site via Doyle Road, and no routes that pass by the site via State Road 415.

Pedestrian Facilities – Currently, sidewalk is provided on the north side of Doyle Road and along both sides of State Road 415 north of Doyle Road along the frontage of the project.

Bicycle Facilities – There are bike lanes along both sides of State Road 415 to the east of the site. There is also the East Central Regional Rail Trail shared-use path (running east-west), located approximately 1,800 feet south of Doyle Road.

CONCLUSIONS

Traffic Engineering Data Solutions, Inc. (TEDS) was retained to analyze the projected traffic impact for the proposed Vineland Reserve development located on the north side of Doyle Road approximately 0.46 miles west of State Road 415 in the City of Deltona, Florida.

Based on the analyses, the existing PM peak-hour two-way volumes for all of the study roadway segments are below the generalized service volume, thereby indicating that all roadway segments currently have acceptable operating conditions. Additionally, all study intersections currently operate acceptably.

In 2017 at buildout of the proposed development, the projected PM peak-hour two-way volumes for all of the study roadway segments are below the generalized service volume, thereby indicating that all roadway segments are projected to have acceptable operating conditions. Additionally, all study intersections are projected to operate acceptably. However, the maximum green times for the eastbound/westbound left-turn movements and eastbound/westbound through movements will need to be adjusted.

Lastly, at the intersection of the development road with Doyle Road, a 205-foot eastbound left-turn lane and a 155-foot westbound right-turn lane are needed.

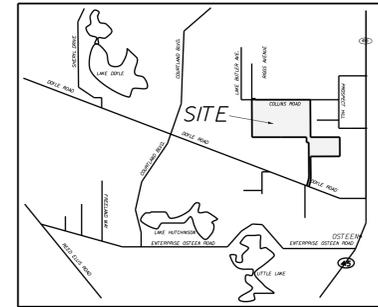
Appendix

PRELIMINARY SITE PLAN

OWNER/DEVELOPER
 LAKE DISTON LANDS, LLC
 300 E NEW YORK AVENUE
 SUITE 300
 DELAND, FLORIDA 32724
 CONTACT: MR. JAMES SWINNER

**PLANNER/ENGINEER/
 ENVIRONMENTAL**
 DEMBERRY
 110 WEST INDIANA AVENUE, SUITE 202
 DELAND, FLORIDA 32720
 CONTACT: TAD W. KASBERG, P.E.
 (386) 785-0468

SURVEYOR
 BLACKWELL AND ASSOCIATES
 995 WEST VOLUSIA AVENUE
 DELAND, FL 32720
 (386) 734-7660



LOCATION MAP
 NOT TO SCALE

LEGEND

DESCRIPTION	EXISTING	PROPOSED
TREE PRESERVATION AREA	(Symbol)	(Symbol)
PROPOSED POND	(Symbol)	(Symbol)
SANITARY SEWER SYSTEM	(Symbol)	(Symbol)
STORM SEWER SYSTEM	(Symbol)	(Symbol)
WATER SYSTEM	(Symbol)	(Symbol)
FIRE HYDRANT	(Symbol)	(Symbol)
LOT NUMBER	(Symbol)	(Symbol)
CONTOUR	(Symbol)	(Symbol)
EXISTING CONTOUR	(Symbol)	(Symbol)
SOIL BOUNDARY	(Symbol)	(Symbol)
VEGETATION BOUNDARY	(Symbol)	(Symbol)

OFFICIAL RECORDS BOOK 5402, PAGE 1532

A PORTION OF THE NORTH 1/4 OF SECTION 12, TOWNSHIP 19 SOUTH, RANGE 31 EAST, VOLUSIA COUNTY, FLORIDA MORE PARTICULARLY DESCRIBED AS FOLLOWS: COMMENCE AT THE NORTH 1/4 CORNER OF SAID SECTION 12; THENCE SOUTH 00°12'06" EAST, 35.00 FEET TO THE POINT OF BEGINNING; THENCE NORTH 89°59'16" EAST, 1333.40 FEET; THENCE SOUTH 00°24'06" EAST, 1285.85 FEET; THENCE NORTH 89°57'56" EAST, 1337.92 FEET; TO A POINT ON THE EAST LINE OF THE NORTHEAST 1/4 OF SAID SECTION 12; THENCE SOUTH 00°35'15" EAST, ALONG SAID LINE, 660.18 FEET; THENCE SOUTH 89°57'15" WEST, 1340.18 FEET; THENCE NORTH 00°32'09" WEST, 480.44 FEET; THENCE SOUTH 89°59'15" WEST, 178.79 FEET; THENCE NORTH 00°23'29" WEST, 180.04 FEET; THENCE SOUTH 89°59'36" WEST, 1440.94 FEET; TO A POINT ON THE EAST LINE OF THE WEST 3/4 ACRES OF THE NORTHEAST 1/4 OF THE NORTHWEST 1/4 OF SAID SECTION 12; THENCE NORTH 00°26'19" WEST, ALONG SAID LINE, 1285.77 FEET; THENCE SOUTH 89°59'21" EAST, 288.37 FEET TO THE POINT OF BEGINNING SAID LANDS LYING IN VOLUSIA COUNTY, FLORIDA.

LESS:

A PORTION OF THE NORTH 1/4 OF SECTION 12, TOWNSHIP 19 SOUTH, RANGE 31 EAST, VOLUSIA COUNTY, FLORIDA MORE PARTICULARLY DESCRIBED AS FOLLOWS: COMMENCE AT THE NORTHEAST CORNER OF SAID SECTION 12; THENCE SOUTH 89°59'26" WEST, ALONG THE NORTH LINE OF THE NORTHEAST 1/4 OF SAID SECTION 12, A DISTANCE OF 2622.03 FEET; THENCE SOUTH 00°12'15" EAST, 1004.55 FEET; THENCE SOUTH 89°47'45" WEST, 104.82 FEET TO THE POINT OF BEGINNING; THENCE SOUTH 00°12'15" EAST, A DISTANCE OF 220.00 FEET; THENCE SOUTH 89°47'45" WEST, 104.82 FEET TO THE POINT OF BEGINNING SAID LANDS LYING IN VOLUSIA COUNTY, FLORIDA.

THE WEST 31 ACRES OF THE NORTHEAST 1/4 OF THE NORTHWEST 1/4 OF SECTION 12, TOWNSHIP 19 SOUTH, RANGE 31 EAST, VOLUSIA COUNTY, FLORIDA, MORE PARTICULARLY DESCRIBED AS FOLLOWS: COMMENCE AT THE NORTH 1/4 CORNER OF SAID SECTION 12; THENCE NORTH 89°59'21" WEST, ALONG THE NORTH LINE OF THE NORTHWEST 1/4 OF SAID SECTION 12, A DISTANCE OF 2883.63 FEET; THENCE SOUTH 00°26'19" WEST, 35.00 FEET TO THE POINT OF BEGINNING; THENCE SOUTH 00°26'19" EAST, A DISTANCE OF 1283.10 FEET; THENCE SOUTH 00°26'19" EAST, 2.68 FEET; THENCE SOUTH 89°59'40" WEST, 1045.57 FEET; THENCE NORTH 00°26'19" WEST, 1321.08 FEET; THENCE SOUTH 89°59'21" EAST, 167.56 FEET; THENCE SOUTH 00°07'39" WEST, 35.00 FEET; THENCE SOUTH 89°59'21" EAST, 878.28 FEET TO THE POINT OF BEGINNING SAID LANDS LYING IN VOLUSIA COUNTY, FLORIDA.

LEGAL DESCRIPTION: BY SURVEYOR

A PORTION OF LAND LYING IN SECTION 12, TOWNSHIP 19 SOUTH, RANGE 31 EAST, VOLUSIA COUNTY, FLORIDA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCE AT THE NORTHEAST CORNER OF SECTION 12, TOWNSHIP 19 SOUTH, RANGE 31 EAST, VOLUSIA COUNTY, FLORIDA; THENCE ALONG THE NORTH LINE OF SECTION 12 A BEARING OF N89°41'20"W, A DISTANCE OF 2667.07 FEET TO THE NORTH QUARTER CORNER OF SECTION 12; THENCE S00°07'18"W, A DISTANCE OF 35.00 FEET TO THE POINT OF BEGINNING; THENCE S89°41'20"E, A DISTANCE OF 1333.40 FEET, SAID LINE BEING 35.00 FEET SOUTH OF AND PARALLEL TO THE NORTH LINE OF SECTION 12; THENCE S00°04'42"E ALONG THE WEST LINE OF JOE SMITH PLACE AS RECORDED IN MAP BOOK 3, PAGE 32, PUBLIC RECORDS OF VOLUSIA COUNTY, FLORIDA, A BEARING OF S89°42'42"E, AND A DISTANCE OF 1337.91 FEET; THENCE ALONG THE WEST LINE OF ASSESSORS SUBDIVISION OF THE R.D. BROOKE PLACE, AS RECORDED IN MAP BOOK 3, PAGE 68, PUBLIC RECORDS OF VOLUSIA COUNTY, FLORIDA, A BEARING OF S00°15'27"E, A DISTANCE OF 660.18 FEET; THENCE ALONG THE NORTH LINE OF THE SOUTH HALF OF THE SOUTHEAST QUARTER OF THE NORTHEAST QUARTER OF SECTION 12 A BEARING OF N89°43'23"W, A DISTANCE OF 1340.10 FEET; THENCE N00°12'45"W, A DISTANCE OF 480.44 FEET; THENCE N89°40'56"W, A DISTANCE OF 178.79 FEET; THENCE N00°04'05"W, A DISTANCE OF 180.04 FEET; THENCE N89°41'00"W, A DISTANCE OF 1440.94 FEET; THENCE N89°40'56"W, A DISTANCE OF 1045.57 FEET; THENCE N00°06'55"W, A DISTANCE OF 1321.08 FEET; THENCE S89°39'57"E, A DISTANCE OF 167.56 FEET; THENCE S00°20'03"W, A DISTANCE OF 35.00 FEET; THENCE S89°39'57"E, A DISTANCE OF 1166.66 FEET TO THE POINT OF BEGINNING.

LESS THE FOLLOWING:

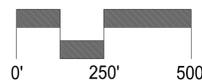
A PORTION OF LAND LYING IN SECTION 12, TOWNSHIP 19 SOUTH, RANGE 31 EAST, VOLUSIA COUNTY, FLORIDA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCE AT THE NORTHEAST CORNER OF SECTION 12, TOWNSHIP 19 SOUTH, RANGE 31 EAST, VOLUSIA COUNTY, FLORIDA; THENCE N89°41'20"W ALONG THE NORTH LINE OF SECTION 12, A DISTANCE OF 2622.03 FEET; THENCE S00°06'59"W, A DISTANCE OF 1004.55 FEET; THENCE N89°55'01"W, A DISTANCE OF 104.82 FEET TO THE POINT OF BEGINNING; THENCE S00°06'59"W, A DISTANCE OF 220.00 FEET; THENCE N89°53'01"W, A DISTANCE OF 200.00 FEET; THENCE N00°06'59"E, A DISTANCE OF 220.00 FEET; THENCE S89°53'01"E, A DISTANCE OF 200.00 FEET TO THE POINT OF BEGINNING.

DISCLAIMER

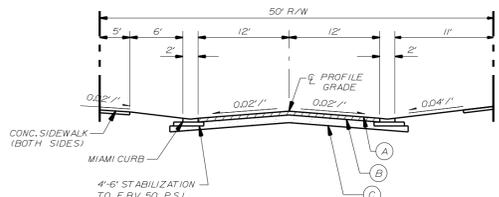
THIS CONCEPTUAL EXHIBIT ILLUSTRATES THE GENERAL INTENT OF THE DEVELOPMENT, AND IS NOT BASED ON AN ENGINEERING ANALYSIS. THE CONCEPT SHOULD NOT BE USED FOR COST PURPOSES AS IT IS SUBJECT TO CHANGE.

10/5/2015



SITE DATA

- PARCEL I.D. - 12-19-31-00-00-0251, 12-19-31-00-00-0151, 12-19-31-00-00-0250, 12-19-31-00-00-0150, 12-19-31-00-00-0061, 12-19-31-00-00-0062, 12-19-31-00-00-0020, 12-19-31-00-00-0030
- 102.7 AC GROSS LAND AREA
- 12.4 AC RETENTION (12.1%)
- 21.2 AC TREE PRESERVATION (25.0%)
- 24.4 AC OPEN AREA (23.8%)
- (35% TO BE COMMON AREA)
- PROPOSED STREET LENGTH = 16,996 LF
- HORIZONTAL ALIGNMENT OF STREETS MEET FLORIDA DEPARTMENT OF TRANSPORTATION MINIMUM STANDARDS FOR A 20 MPH DESIGN SPEED
- 407 TOTAL LOTS
- 279 SINGLE FAMILY
- 128 TOWNHOME
- ZONING - RPUD
- SINGLE FAMILY:
- SETBACKS: FRONT 20', SIDE 15', REAR 20'
- MIN. LOT SIZE: 6,900 SF
- MIN. LOT WIDTH: 50'
- TOWNHOME:
- SETBACKS: FRONT 20', REAR 20'
- MIN. LOT SIZE: 2,520 SF
- MIN. LOT WIDTH: 24'
- PROPOSED DENSITY = 4.0 UNITS/ACRE
- GEOMETRY IS PURELY CONCEPTUAL



TYPICAL SECTION

SOILS LEGEND

01	APOPKA FINE SAND
04	ASTATULA FINE SAND, 0-8 PERCENT SLOPES
05	ASTATULA FINE SAND
11	DAYTONA SAND, 0-5 PERCENT SLOPES
22	ELECTRA FINE SAND
37	ORSDINO FINE SAND
42	PAOLA FINE SAND, 8-17 PERCENT SLOPES
63	TAVARES FINE SAND
99	NATURAL VEGETATION RETENTION AREAS WATER

VEGETATION TABLE

100	GENERALIZED URBAN
200	GENERALIZED AGRICULTURE
210	ABANDONED FIELDS
412	PINE/FAERIC OAK
414	PINE/AMERIC OAK
421	KERIC OAK
710	DISTURBED LANDS

**VINELAND RESERVE
 MASTER DEVELOPMENT PLAN
 CITY OF DELTONA**

10/5/2015 1:39:29 PM C:\Users\jswin\Documents\VINELAND_MASTER_PLAN.dwg

LEGEND

DESCRIPTION	EXISTING	PROPOSED
TREE PRESERVATION AREA	(Symbol)	(Symbol)
PROPOSED POND	(Symbol)	(Symbol)
SANITARY SEWER SYSTEM	(Symbol)	(Symbol)
STORM SEWER SYSTEM	(Symbol)	(Symbol)
WATER SYSTEM	(Symbol)	(Symbol)
FIRE HYDRANT	(Symbol)	(Symbol)
LOT NUMBER	(Symbol)	(Symbol)
CONTOUR	(Symbol)	(Symbol)
EXISTING CONTOUR	(Symbol)	(Symbol)
SOIL BOUNDARY	(Symbol)	(Symbol)
VEGETATION BOUNDARY	(Symbol)	(Symbol)

SOILS LEGEND

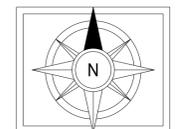
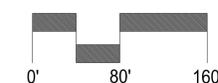
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04	ASTATULA FINE SAND, 0-8 PERCENT SLOPES
05	ASTATULA FINE SAND
17	DAYTONA SAND, 0-5 PERCENT SLOPES
22	ELECTRA FINE SAND
37	ORSINO FINE SAND
42	PAOLA FINE SAND, 8-17 PERCENT SLOPES
63	TAVARES FINE SAND
99	OPEN WATER

VEGETATION TABLE

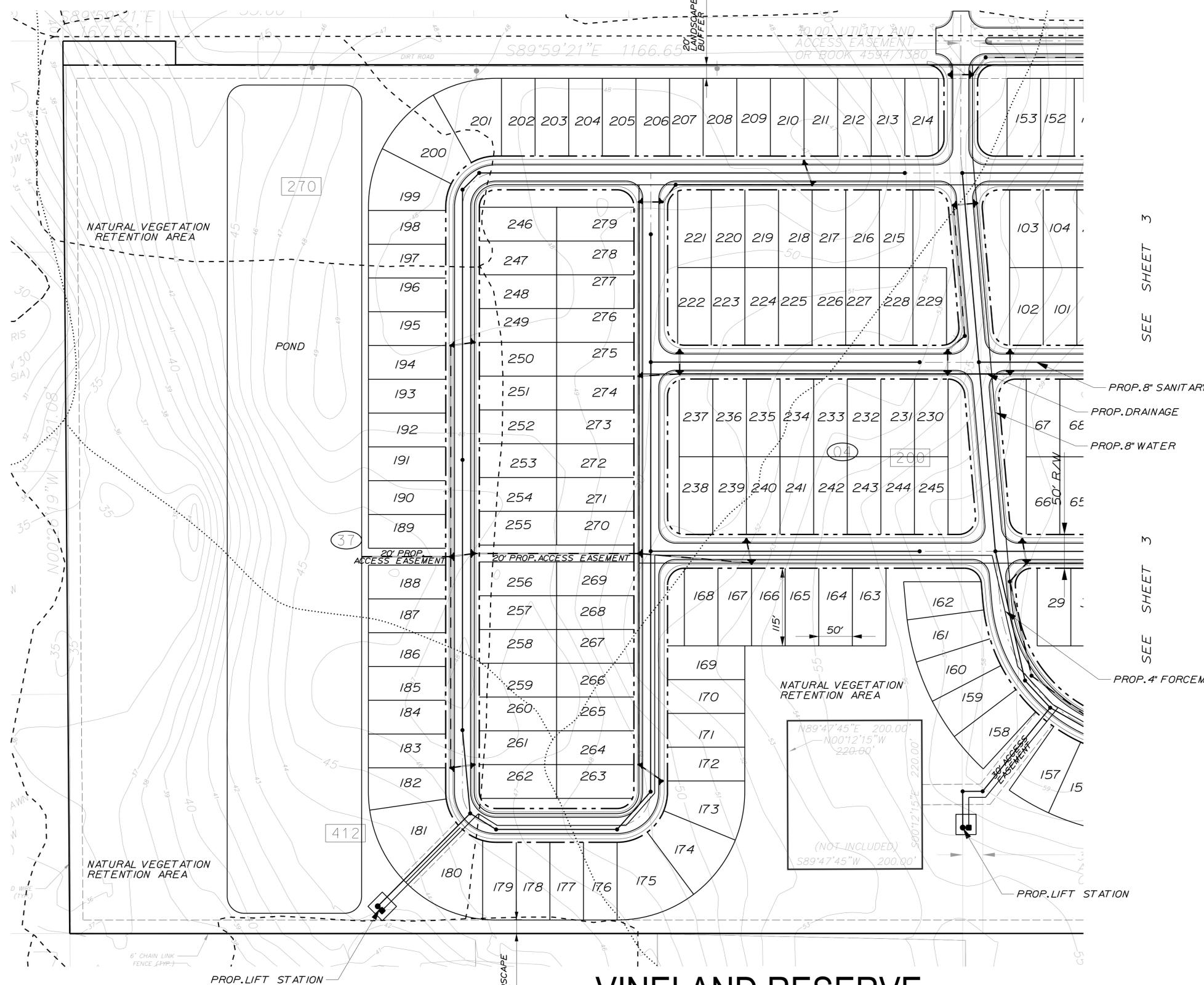
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200	GENERALIZED AGRICULTURE
270	ABANDONED FIELDS
412	PINE/XERIC OAK
414	PINE/MESIC OAK
421	XERIC OAK
740	DISTURBED LANDS

DISCLAIMER
 THIS CONCEPTUAL EXHIBIT ILLUSTRATES THE GENERAL INTENT OF THE DEVELOPMENT, AND IS NOT BASED ON AN ENGINEERING ANALYSIS. THE CONCEPT SHOULD NOT BE USED FOR COST PURPOSES AS IT IS SUBJECT TO CHANGE.

10/5/2015



VINELAND RESERVE
 MASTER DEVELOPMENT PLAN
 CITY OF DELTONA

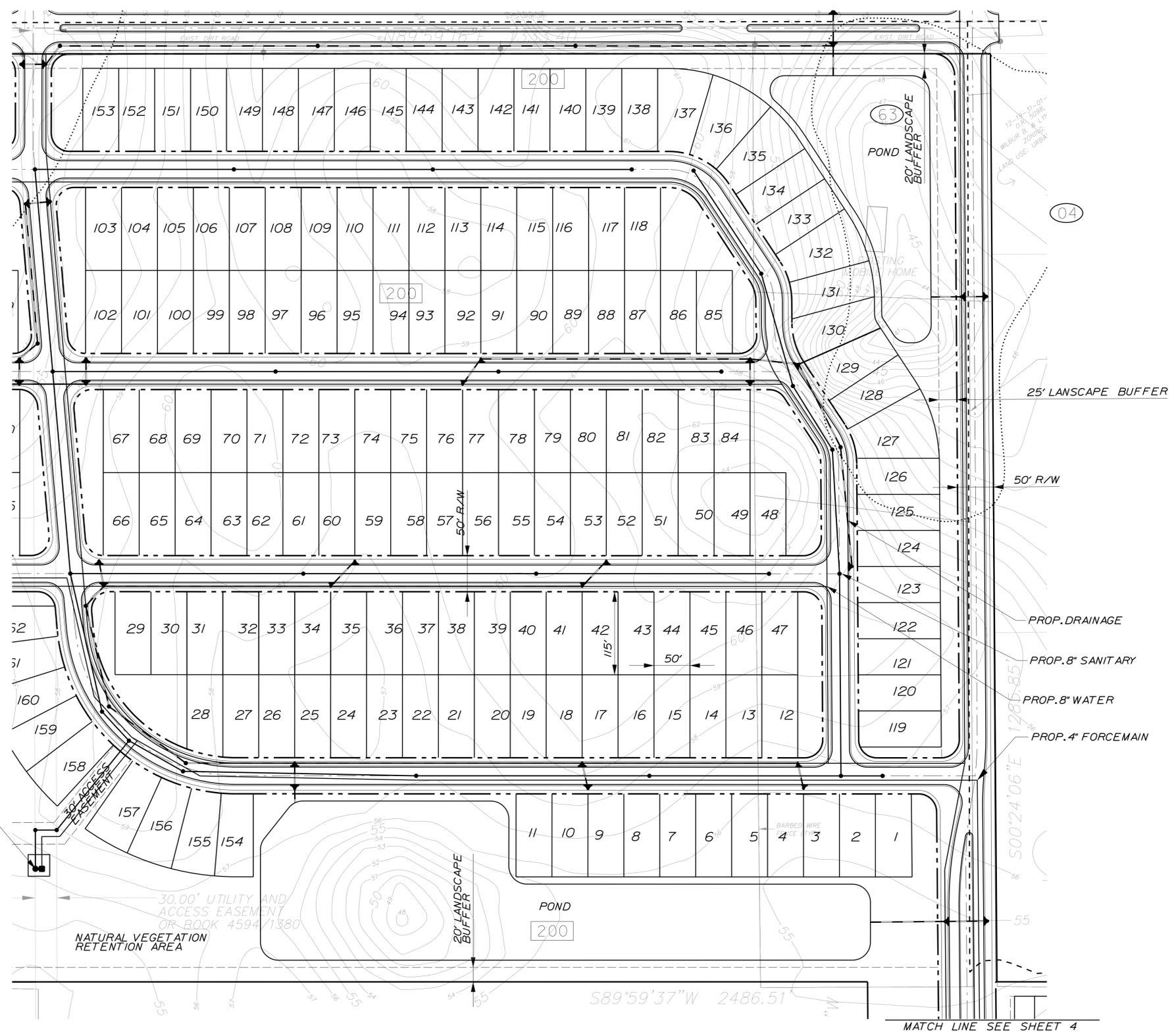


SEE SHEET 3

SEE SHEET 3

SEE SHEET 2

SEE SHEET 2



LEGEND

DESCRIPTION	EXISTING	PROPOSED
TREE PRESERVATION AREA		
PROPOSED POND		
SANITARY SEWER SYSTEM		
STORM SEWER SYSTEM		
WATER SYSTEM		
FIRE HYDRANT		
LOT NUMBER		
CONTOUR		
EXISTING CONTOUR		
SOIL BOUNDARY		
VEGETATION BOUNDARY		

SOILS LEGEND

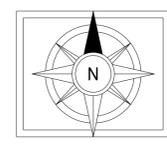
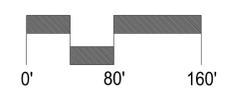
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05	ASTATULA FINE SAND
17	DAYTONA SAND, 0-5 PERCENT SLOPES
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37	ORSINO FINE SAND
42	PAOLA FINE SAND, 8-17 PERCENT SLOPES
63	TAVARES FINE SAND
99	OPEN WATER

VEGETATION TABLE

100	GENERALIZED URBAN
200	GENERALIZED AGRICULTURE
270	ABANDONED FIELDS
412	PINE/XERIC OAK
414	PINE/MESIC OAK
421	XERIC OAK
740	DISTURBED LANDS

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10/5/2015



VINELAND RESERVE
 MASTER DEVELOPMENT PLAN
 CITY OF DELTONA

LEGEND

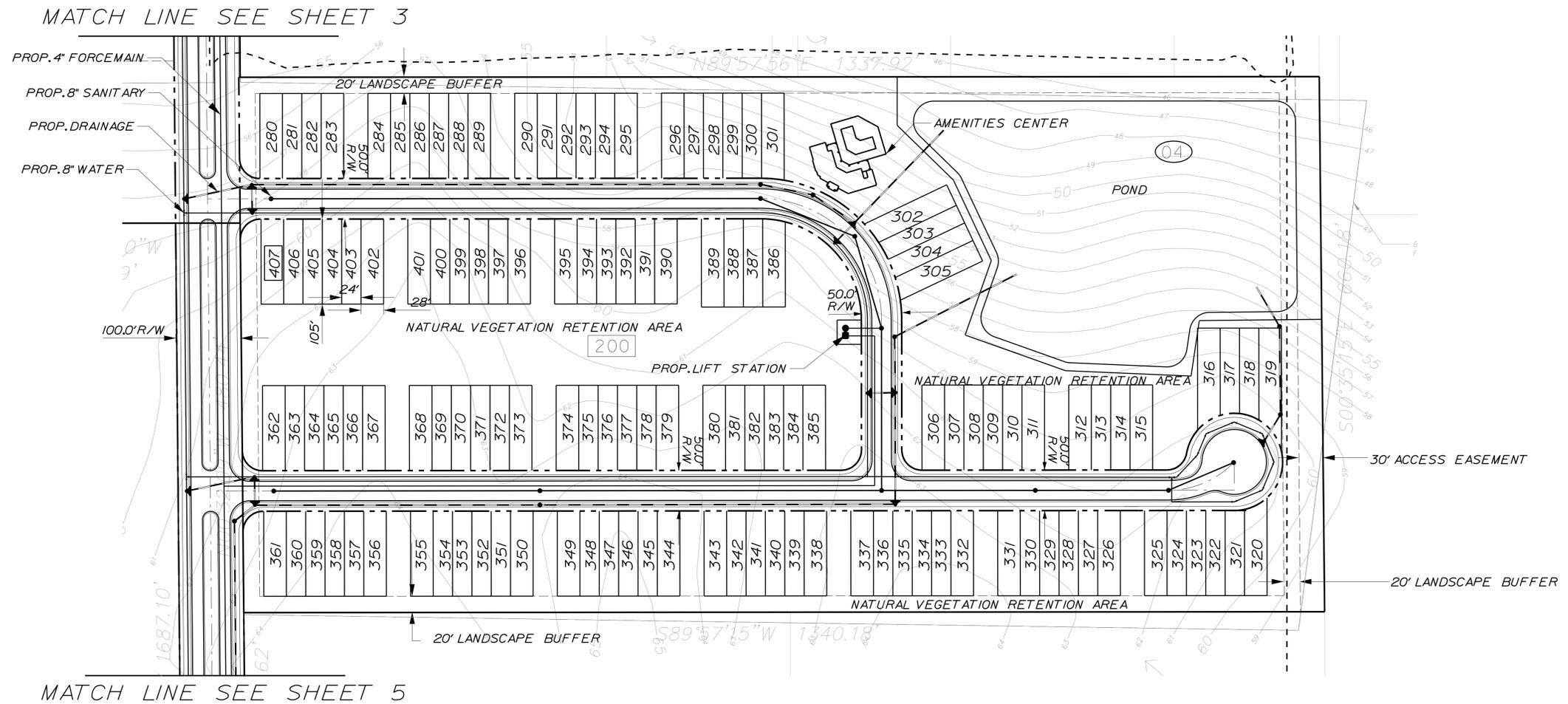
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TREE PRESERVATION AREA		
PROPOSED POND		
SANITARY SEWER SYSTEM		
STORM SEWER SYSTEM		
WATER SYSTEM		
FIRE HYDRANT		
LOT NUMBER		
CONTOUR		
EXISTING CONTOUR		
SOIL BOUNDARY		
VEGETATION BOUNDARY		

SOILS LEGEND

01	APOPKA FINE SAND
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63	TAVARES FINE SAND
99	OPEN WATER

VEGETATION TABLE

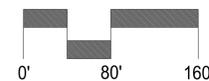
100	GENERALIZED URBAN
200	GENERALIZED AGRICULTURE
270	ABANDONED FIELDS
412	PINE/XERIC OAK
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740	DISTURBED LANDS



VINELAND RESERVE
 MASTER DEVELOPMENT PLAN
 CITY OF DELTONA

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10/5/2015



LEGEND

DESCRIPTION	EXISTING	PROPOSED
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PROPOSED POND		
SANITARY SEWER SYSTEM		
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WATER SYSTEM		
FIRE HYDRANT		
LOT NUMBER		
CONTOUR		
EXISTING CONTOUR		
SOIL BOUNDARY		
VEGETATION BOUNDARY		

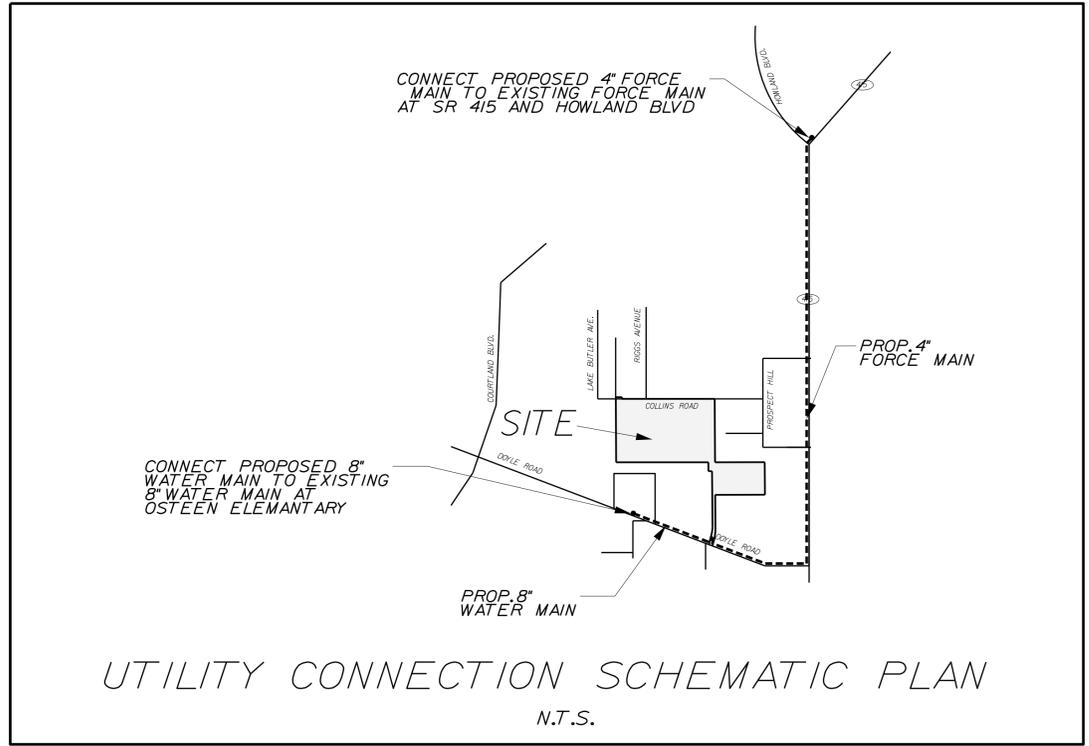
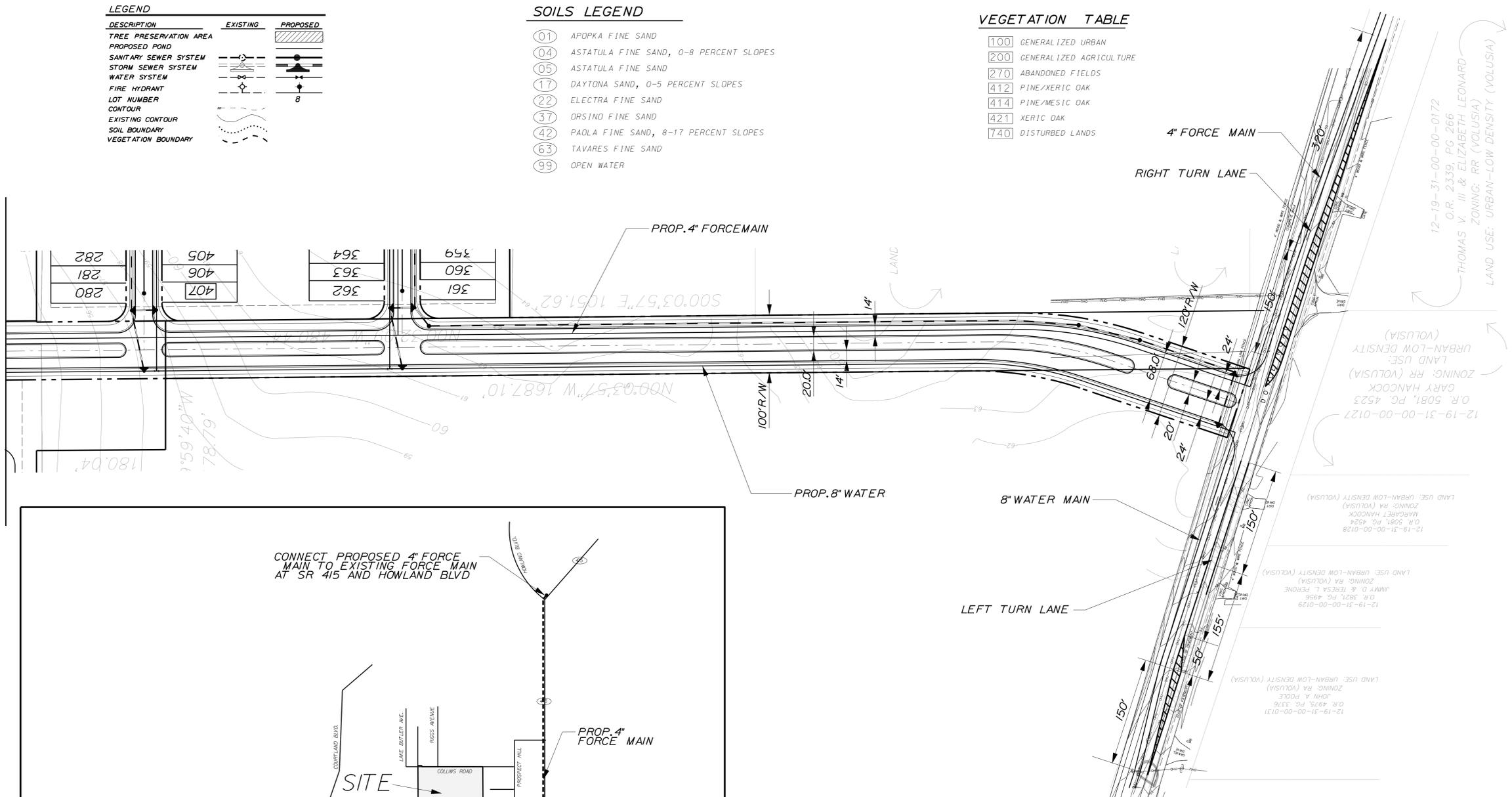
SOILS LEGEND

01	APOPKA FINE SAND
04	ASTATULA FINE SAND, 0-8 PERCENT SLOPES
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63	TAVARES FINE SAND
99	OPEN WATER

VEGETATION TABLE

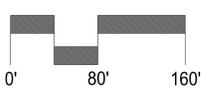
100	GENERALIZED URBAN
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270	ABANDONED FIELDS
412	PINE/XERIC OAK
414	PINE/MESIC OAK
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740	DISTURBED LANDS

MATCH LINE SEE SHEET 4



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10/5/2015



VINELAND RESERVE
MASTER DEVELOPMENT PLAN
CITY OF DELTONA

TRAFFIC METHODOLOGY AND REVIEW CORRESPONDENCE



Ref: 10838

TECHNICAL MEMORANDUM

To: Mr. James Skinner
From: Chris J. Walsh, P.E.
Subject: Traffic Impact Analysis Methodology – Vineland Reserve
Deltona, Florida
Date: October 28, 2015

Introduction

Traffic Engineering Data Solutions, Inc. (TEDS) has been retained to conduct a traffic impact analysis for the Vineland Reserve development located on the north side of Doyle Road approximately 0.46 miles west of State Road 415 in the City of Deltona, Florida (see **Figure 1**). The proposed development will include 279 single-family residential units and 128 townhomes. A preliminary site plan of the proposed development is attached. This letter summarizes the methodology for the traffic impact analysis.

Trip Generation

The total daily and PM peak-hour trip generation potential for the development was determined based on trip generation equations and rates provided in the Institute of Transportation Engineer's (ITE) *Trip Generation Manual, 9th Edition*. For single-family residential units, Land Use Code 210 (Single-Family Detached Housing) was used. For townhomes, Land Use Code 230 (Residential Condominiums/Townhouse) was used. As summarized in **Table 1**, the proposed development is projected to generate 3,497 total daily trips (1,749 in, 1,748 out) and 338 total PM peak-hour trips (216 in, 122 out).



Aerial Image: Google Earth



Figure 1
Site Location Map

**Table 1
 Trip Generation Summary**

Land Use	Intensity	Units	Daily			PM Peak		
			In	Out	Total	In	Out	Total
Single-Family Detached Housing	279	Dwelling Units	1350	1,349	2,699	167	98	265
Residential Condominium/Townhouse	128	Dwelling Units	399	398	797	49	25	74
Net New External Trips			1,749	1,748	3,497	216	122	339

Single-Family Detached Housing (ITE 9th Edition - Land Use Code 210)

Daily LN (T) = 0.92 x LN (# of DU) + 2.72 50% In 50% Out
 PM Peak Hour LN (T) = 0.90 x LN (# of DU) + 0.51 63% In 37% Out

Residential Condominium/Townhouse (ITE 9th Edition - Land Use Code 230)

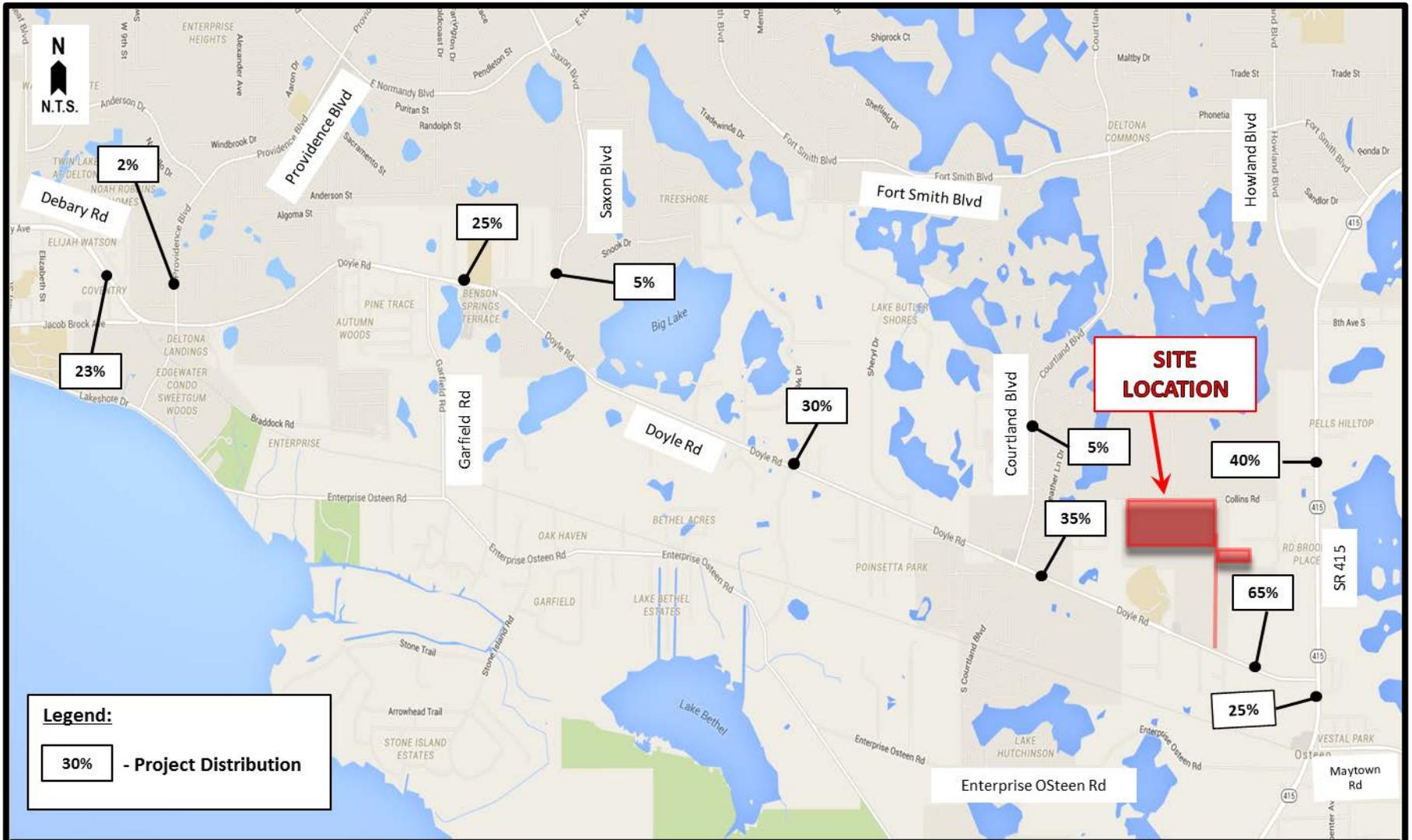
Daily LN (T) = 0.87 x LN (# of DU) + 2.46 50% In 50% Out
 PM Peak Hour LN (T) = 0.82 x LN (# of DU) + 0.32 67% In 33% Out

Trip Distribution & Assignment

A traffic impact analysis was previously conducted in 2006 for the Collingswood Properties and Vineland Reserve developments. The distribution included in the approved TIA included was based on a model distribution. Recognizing that the Vineland Reserve development is still proposed as a residential development, the model distribution from the 2006 TIA was used as the basis for the newly proposed distribution. The proposed trip distribution is shown in **Figure 2** and the 2006 model distribution is attached.

Study Area

Because the proposed development is projected to generate more than 300 two-way peak hour external trips, the study area was determined based upon a five-percent level of significance as consistent with the River to Sea Transportation Planning Organization (River to Sea TPO) Transportation Impact Analysis (TIA) Guidelines. A summary of the determination of the five-percent significance area can be found in **Table 2**. The adopted levels of service (LOS) included in **Table 2** were obtained from the City of Deltona’s comprehensive plan and the generalized service volumes based on FDOT’s 2013 Quality/Level of Service Handbook.



Aerial Image: Google Earth



Figure 4
Trip Distribution Map

Table 2
Summary of Significant Impact Determination

Roadway Segment	Existing Number of Lanes	Adopted Level of Service Standard	Pk-Hr 2-Way Generalized Service Volume	Percent Assignment	Pk-Hr 2-Way Project Trips	Project Trips as % of Service Volume	Impact Exceeds 5%?
State Road 415							
Enterprise-Osteen Road to Doyle Rd	4	E	3,759	25.0%	85	2.26%	No
Doyle Rd to Walmart Dwy	4	E	3,759	40.0%	135	3.59%	No
Doyle Road							
Main Street to Providence Blvd	4	E	3,759	23.0%	78	2.08%	No
Providence Blvd to Garfield Road	2	E	1,230	25.0%	85	6.91%	YES
Garfield Road to Saxon Blvd	2	E	1,230	25.0%	85	6.91%	YES
Saxon Blvd to Courtland Blvd	2	E	1,230	30.0%	101	8.21%	YES
Courtland Blvd to Project Access Dwy	2	E	1,230	35.0%	118	9.59%	YES
Project Access Dwy to SR 415	2	E	1,230	65.0%	220	17.89%	YES
Courtland Boulevard							
Fort Smith Blvd to Doyle Road	2	E	1,150	5.0%	17	1.48%	No
Saxon Boulevard							
Normandy Blvd to Doyle Road	2	E	1,230	5.0%	17	1.38%	No
Providence Boulevard							
Anderson Dr to Doyle Road	2	E	1,020	2.0%	7	0.69%	No

Based on **Table 2**, the following roadway segments will be analyzed:

- Doyle Road, from Providence Boulevard to State Road 415

The study intersections will include the following:

- Doyle Road at Providence Boulevard
- Doyle Road at Saxon Boulevard
- Doyle Road at Courtland Boulevard
- Doyle Road at State Road 415

The PM peak-hour background traffic volumes for the roadway segments will be projected by factoring up existing volumes through the application of vested trips from the City of Deltona, and/or historical growth rates. Project trips will then be added to the future background volumes to project the build out conditions for each roadway segment and intersection.

The existing and future roadway segment and intersection operating conditions will be analyzed for the PM peak hour. The roadway segments will be analyzed by comparing the two-way link volumes to the generalized service volumes. Should the projected volume be less than the generalized service volume then it shall be concluded that the roadway will operate at an acceptable level of service (LOS)

volume then it shall be concluded that the roadway will operate at an acceptable level of service (LOS) standard at build out of the project. In the event the future volume of a roadway exceeds the generalized service volume, TEDS may conduct a more detailed highway/arterial analysis to further refine the level of service evaluation.

Existing and future PM peak-hour intersection operating conditions will be analyzed using the Highway Capacity Software based upon the committed geometry. Existing signal timings and phasing will be used for intersection analyses. A study intersection will be deemed to operate acceptably if the overall intersection LOS meets the adopted LOS standard for the roadways. Per the River to Sea TPO TIA Guidelines, in the event the two intersecting roadways have different LOS standards, then the lower standard shall prevail. For example, if one roadway has a LOS standard of D and the intersecting road has a LOS standard of E, then the overall intersection LOS standard shall be E.

Critical and Near Critical Study Area

A critical, near critical, and hurricane critical roadway segment is one where the existing daily volume is 90 percent or more of a roadway's service volume at the adopted LOS standard. There are no critical, near critical, and hurricane critical roadway segments identified on Volusia County Traffic Engineering Division's critical/near-critical map within a five-mile travel distance.

Alternative Mode Analysis

Per the River to Sea TPO TIA Guidelines, an evaluation relating to transit, pedestrian, and bicycle facilities will be provided.

Conclusions, Recommendations and Mitigation

Based upon the results of the analysis, conclusions and recommendations will be prepared. If the TIA identifies deficient roadways/intersections and the project's impacts are non-deminimus, then a plan to mitigate the project's impacts will be provided.

From: Chris Walsh [<mailto:cwalsh@teds-fl.com>]

Sent: Tuesday, November 03, 2015 11:02 AM

To: 'Ron Paradise' <RParadise@deltonafl.gov>

Cc: 'Chris Bowley' <CBowley@deltonafl.gov>; 'Kat Kyp' <KKyp@deltonafl.gov>; 'Melissa Winsett' (mwinsett@volusia.org) <mwinsett@volusia.org>

Subject: RE: Vineland Reserve

Good afternoon Ron,

Below are responses to your methodology comments (see in red). Please note that based on comment #4, we have prepared a revised distribution and included it in a revised methodology (basically the only change is the distribution and the significance test table). Also attached is the old approved traffic study for the development which was the basis of the trip distribution in our first methodology.

Melissa, a driveway permit will be needed from the County for a connection to Doyle Road. Attached is the site plan.

Chris

Chris J. Walsh, PE
Senior Transportation Engineer



Traffic Engineering Data Solutions, Inc.
80 Spring Vista Drive
DeBary, Florida 32713
386.753.0558 (o) 386.801.5682 (c)
cwalsh@teds-fl.com
www.teds-fl.com

From: Ron Paradise [<mailto:RParadise@deltonafl.gov>]

Sent: Thursday, October 29, 2015 11:44 AM

To: Chris Walsh <cwalsh@teds-fl.com>

Cc: Chris Bowley <CBowley@deltonafl.gov>; Kat Kyp <KKyp@deltonafl.gov>; Melissa Winsett (mwinsett@volusia.org) <mwinsett@volusia.org>

Subject: RE: Vineland Reserve

Mr. Walsh, I looked at the methodology and have the following observations/comments?

- 1) The TIA will comport with TPO guidelines – correct? **Correct**
- 2) Need to incorporate some sort of signal warrant oriented analysis at the project entrance and Doyle. Furthermore what improvements will be needed to make that intersection function (i.e. turn lanes, storage, etc.)? **We will first analyze the intersection under STOP sign control. If the intersection operates acceptably, then a signal warrant analysis will not be conducted. If the intersection fails (sidestreet approach does not meet the LOS standard), then a signal warrant analysis will be conducted.**
- 3) Please coordinate with Volusia County Traffic Engineering (Doyle is a County Rd.). **Will do**
- 4) With regard to the traffic splits, what is the rationale behind only 8% of the trips heading south on SR 415? In addition, what percent of traffic will use Courtland Blvd. from Doyle to Ft. Smith? Only 2%? **We had utilized the distribution from the previously approved TIA for the same development (see attachment). However, we are in**

agreement as we would expect more traffic to be to/from the south on SR 415. Based on local knowledge, we propose a revised (new) distribution....see the attached revised methodology.

- 5) Note: the City does have 2015 traffic counts on City t-fare facilities. Will send under separate cover. Thank you, we received your email with the count data.
- 6) If traffic deficiencies are noted, please indicate how those deficiencies will be addressed and by what entity including a funding mechanism. We will utilize mitigation procedures in accordance with the TIA guidelines and City's LDR's (proportionate-share is the typical approach but will work with the City and County on mitigation solutions).

Have a good day.

Ron

From: Chris Walsh [<mailto:cwalsh@teds-fl.com>]
Sent: Thursday, October 29, 2015 10:41 AM
To: Ron Paradise
Subject: Vineland Reserve

Good afternoon Ron,

Attached is a methodology for the TIA for the Vineland Reserve development. Please let me know if you have any questions.

Chris

Chris J. Walsh, PE
Senior Transportation Engineer



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80 Spring Vista Drive
DeBary, Florida 32713
386.753.0558 (o) 386.801.5682 (c)
cwalsh@teds-fl.com
www.teds-fl.com

From: Vischal Persaud [<mailto:vpersaud@teds-fl.com>]
Sent: Wednesday, October 28, 2015 2:41 PM
To: cwalsh@teds-fl.com
Subject: Vineland Reserve

Vischal Persaud, EI
Transportation Analyst



Traffic Engineering Data Solutions, Inc.
80 Spring Vista Drive
DeBary, Florida 32713
386.753.0558 (o) 386.753.0778 (f)
vpersaud@teds-fl.com
www.teds-fl.com

Florida has a very broad Public Records Law. Virtually all written communications to or from State and Local Officials and employees are public records available to the public and media upon request. The City of Deltona's policy does not differentiate between personal and business emails. This means email messages, including your e-mail address and any attachments and information we receive online might be disclosed to any person or media making a public records request. E-mail sent on the City system will be considered public and will only be withheld from disclosure if deemed confidential or exempt pursuant to State Law. If you are an individual whose identifying information is exempt under 119.071, Florida Statutes, please so indicate in your email or other communication. If you have any questions about the Florida public records law refer to Chapter 119 Florida Statutes.

TRAFFIC COUNT DATA PRINTOUTS

Groups Printed- All Vehicles

Start Time	PROVIDENCE Northbound					PROVIDENCE Southbound					DOYLE Eastbound					DOYLE Westbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
04:00 PM	22	37	12	0	71	14	25	59	0	98	114	142	44	0	300	8	65	11	0	84	553
04:15 PM	31	27	8	0	66	10	34	74	0	118	123	215	68	0	406	8	65	18	0	91	681
04:30 PM	36	40	6	0	82	12	26	62	0	100	175	230	71	2	478	1	89	26	0	116	776
04:45 PM	29	37	7	0	73	11	27	68	0	106	131	231	62	1	425	6	81	9	0	96	700
Total	118	141	33	0	292	47	112	263	0	422	543	818	245	3	1609	23	300	64	0	387	2710
05:00 PM	31	55	9	0	95	12	37	54	0	103	138	199	53	1	391	9	65	10	0	84	673
05:15 PM	20	44	7	0	71	14	23	57	0	94	191	233	65	1	490	5	60	31	3	99	754
05:30 PM	25	64	16	0	105	14	32	53	0	99	159	215	60	1	435	6	65	17	0	88	727
05:45 PM	26	58	8	0	92	7	23	62	2	94	150	180	65	0	395	3	63	16	0	82	663
Total	102	221	40	0	363	47	115	226	2	390	638	827	243	3	1711	23	253	74	3	353	2817
Grand Total	220	362	73	0	655	94	227	489	2	812	1181	1645	488	6	3320	46	553	138	3	740	5527
Apprch %	33.6	55.3	11.1	0		11.6	28	60.2	0.2		35.6	49.5	14.7	0.2		6.2	74.7	18.6	0.4		
Total %	4	6.5	1.3	0	11.9	1.7	4.1	8.8	0	14.7	21.4	29.8	8.8	0.1	60.1	0.8	10	2.5	0.1	13.4	

Start Time	PROVIDENCE Northbound					PROVIDENCE Southbound					DOYLE Eastbound					DOYLE Westbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:30 PM																					
04:30 PM	36	40	6	0	82	12	26	62	0	100	175	230	71	2	478	1	89	26	0	116	776
04:45 PM	29	37	7	0	73	11	27	68	0	106	131	231	62	1	425	6	81	9	0	96	700
05:00 PM	31	55	9	0	95	12	37	54	0	103	138	199	53	1	391	9	65	10	0	84	673
05:15 PM	20	44	7	0	71	14	23	57	0	94	191	233	65	1	490	5	60	31	3	99	754
Total Volume	116	176	29	0	321	49	113	241	0	403	635	893	251	5	1784	21	295	76	3	395	2903
% App. Total	36.1	54.8	9	0		12.2	28	59.8	0		35.6	50.1	14.1	0.3		5.3	74.7	19.2	0.8		
PHF	.806	.800	.806	.000	.845	.875	.764	.886	.000	.950	.831	.958	.884	.625	.910	.583	.829	.613	.250	.851	.935

Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1

Peak Hour for Each Approach Begins at:

	05:00 PM					04:15 PM					04:30 PM					04:30 PM					
+0 mins.	31	55	9	0	95	10	34	74	0	118	175	230	71	2	478	1	89	26	0	116	
+15 mins.	20	44	7	0	71	12	26	62	0	100	131	231	62	1	425	6	81	9	0	96	
+30 mins.	25	64	16	0	105	11	27	68	0	106	138	199	53	1	391	9	65	10	0	84	
+45 mins.	26	58	8	0	92	12	37	54	0	103	191	233	65	1	490	5	60	31	3	99	
Total Volume	102	221	40	0	363	45	124	258	0	427	635	893	251	5	1784	21	295	76	3	395	
% App. Total	28.	60.	11	0		10.	29	60.	0		35.	50.	14.	0.3		5.3	74.	19.	0.8		
	1	9				5	4				6	1	1			7	2				
PHF	.82	.86	.62	.00	.864	.93	.83	.87	.00	.905	.83	.95	.88	.62	.910	.58	.82	.61	.25		.851
	3	3	5	0		8	8	2	0		1	8	4	5		3	9	3	0		

Groups Printed- Heavy Trucks

Start Time	PROVIDENCE Northbound					PROVIDENCE Southbound					DOYLE Eastbound					DOYLE Westbound					Int. Total	
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total		
04:00 PM	1	0	1	0	2	1	0	1	0	2	5	2	0	0	7	0	2	0	0	2	13	
04:15 PM	0	0	1	0	1	0	0	1	0	1	0	1	0	0	1	0	3	1	0	0	4	7
04:30 PM	0	1	0	0	1	0	0	1	0	1	1	2	0	0	3	0	0	0	0	0	0	5
04:45 PM	0	0	0	0	0	0	1	0	0	1	1	0	0	1	2	0	0	0	0	0	0	3
Total	1	1	2	0	4	1	1	3	0	5	7	5	0	1	13	0	5	1	0	6	28	
05:00 PM	0	2	0	0	2	0	1	0	0	1	0	1	0	0	1	0	0	0	0	0	0	4
05:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	1	0	1	0	0	1	2	
*** BREAK ***																						
05:45 PM	0	0	0	0	0	0	0	0	2	2	0	0	1	0	1	0	1	0	0	1	4	
Total	0	2	0	0	2	0	1	0	2	3	0	1	1	1	3	0	2	0	0	2	10	
Grand Total	1	3	2	0	6	1	2	3	2	8	7	6	1	2	16	0	7	1	0	8	38	
Apprch %	16.7	50	33.3	0		12.5	25	37.5	25		43.8	37.5	6.2	12.5		0	87.5	12.5	0			
Total %	2.6	7.9	5.3	0	15.8	2.6	5.3	7.9	5.3	21.1	18.4	15.8	2.6	5.3	42.1	0	18.4	2.6	0	21.1		

Start Time	PROVIDENCE Northbound					PROVIDENCE Southbound					DOYLE Eastbound					DOYLE Westbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:00 PM																					
04:00 PM	1	0	1	0	2	1	0	1	0	2	5	2	0	0	7	0	2	0	0	2	13
04:15 PM	0	0	1	0	1	0	0	1	0	1	0	1	0	0	1	0	3	1	0	4	7
04:30 PM	0	1	0	0	1	0	0	1	0	1	1	2	0	0	3	0	0	0	0	0	5
04:45 PM	0	0	0	0	0	0	1	0	0	1	1	0	0	1	2	0	0	0	0	0	3
Total Volume	1	1	2	0	4	1	1	3	0	5	7	5	0	1	13	0	5	1	0	6	28
% App. Total	25	25	50	0		20	20	60	0		53.8	38.5	0	7.7		0	83.3	16.7	0		
PHF	.250	.250	.500	.000	.500	.250	.250	.750	.000	.625	.350	.625	.000	.250	.464	.000	.417	.250	.000	.375	.538

Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1

Peak Hour for Each Approach Begins at:

	04:00 PM					04:00 PM					04:00 PM					04:00 PM				
+0 mins.	1	0	1	0	2	1	0	1	0	2	5	2	0	0	7	0	2	0	0	2
+15 mins.	0	0	1	0	1	0	0	1	0	1	0	1	0	0	1	0	3	1	0	4
+30 mins.	0	1	0	0	1	0	0	1	0	1	1	2	0	0	3	0	0	0	0	0
+45 mins.	0	0	0	0	0	0	1	0	0	1	1	0	0	1	2	0	0	0	0	0
Total Volume	1	1	2	0	4	1	1	3	0	5	7	5	0	1	13	0	5	1	0	6
% App. Total	25	25	50	0		20	20	60	0		53.8	38.5	0	7.7		0	83.3	16.7	0	
PHF	.25	.25	.50	.00	.500	.25	.25	.75	.00	.625	.35	.62	.00	.25	.464	.00	.41	.25	.00	.375
	0	0	0	0		0	0	0	0		0	5	0	0		0	7	0	0	

Groups Printed- ALL VEHICLES

Start Time	SAXON BLVD Northbound					SAXON BLVD Southbound					DOYLE RD Eastbound					DOYLE RD Westbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
04:00 PM	0	0	1	0	1	13	0	21	0	34	41	90	0	0	131	1	48	23	0	72	238
04:15 PM	0	0	0	0	0	20	0	20	0	40	44	73	2	0	119	0	56	19	0	75	234
04:30 PM	0	0	0	0	0	12	0	17	0	29	49	107	0	0	156	0	65	23	0	88	273
04:45 PM	0	0	0	0	0	16	0	16	0	32	44	79	0	0	123	0	43	26	0	69	224
Total	0	0	1	0	1	61	0	74	0	135	178	349	2	0	529	1	212	91	0	304	969
05:00 PM	0	0	1	0	1	15	1	23	0	39	53	100	0	0	153	0	48	23	0	71	264
05:15 PM	0	0	0	0	0	11	0	28	0	39	59	97	0	0	156	0	42	36	0	78	273
05:30 PM	0	0	0	0	0	12	0	31	0	43	54	100	1	0	155	2	48	31	0	81	279
05:45 PM	0	0	0	0	0	17	0	22	0	39	46	91	0	0	137	0	52	28	0	80	256
Total	0	0	1	0	1	55	1	104	0	160	212	388	1	0	601	2	190	118	0	310	1072
Grand Total	0	0	2	0	2	116	1	178	0	295	390	737	3	0	1130	3	402	209	0	614	2041
Apprch %	0	0	100	0		39.3	0.3	60.3	0		34.5	65.2	0.3	0		0.5	65.5	34	0		
Total %	0	0	0.1	0	0.1	5.7	0	8.7	0	14.5	19.1	36.1	0.1	0	55.4	0.1	19.7	10.2	0	30.1	

Start Time	SAXON BLVD Northbound					SAXON BLVD Southbound					DOYLE RD Eastbound					DOYLE RD Westbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
05:00 PM	0	0	1	0	1	15	1	23	0	39	53	100	0	0	153	0	48	23	0	71	264
05:15 PM	0	0	0	0	0	11	0	28	0	39	59	97	0	0	156	0	42	36	0	78	273
05:30 PM	0	0	0	0	0	12	0	31	0	43	54	100	1	0	155	2	48	31	0	81	279
05:45 PM	0	0	0	0	0	17	0	22	0	39	46	91	0	0	137	0	52	28	0	80	256
Total Volume	0	0	1	0	1	55	1	104	0	160	212	388	1	0	601	2	190	118	0	310	1072
% App. Total	0	0	100	0		34.4	0.6	65	0		35.3	64.6	0.2	0		0.6	61.3	38.1	0		
PHF	.000	.000	.250	.000	.250	.809	.250	.839	.000	.930	.898	.970	.250	.000	.963	.250	.913	.819	.000	.957	.961

Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1

Peak Hour for Each Approach Begins at:	04:00 PM					05:00 PM					05:00 PM					05:00 PM				
+0 mins.	0	0	1	0	1	15	1	23	0	39	53	100	0	0	153	0	48	23	0	71
+15 mins.	0	0	0	0	0	11	0	28	0	39	59	97	0	0	156	0	42	36	0	78
+30 mins.	0	0	0	0	0	12	0	31	0	43	54	100	1	0	155	2	48	31	0	81
+45 mins.	0	0	0	0	0	17	0	22	0	39	46	91	0	0	137	0	52	28	0	80
Total Volume	0	0	1	0	1	55	1	104	0	160	212	388	1	0	601	2	190	118	0	310
% App. Total	0	0	100	0		34.4	0.6	65	0		35.3	64.6	0.2	0		0.6	61.3	38.1	0	
PHF	.000	.000	.250	.000	.250	.809	.250	.839	.000	.930	.898	.970	.250	.000	.963	.250	.913	.819	.000	.957

Groups Printed- Heavy Trucks

Start Time	SAXON BLVD Northbound					SAXON BLVD Southbound					DOYLE RD Eastbound					DOYLE RD Westbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	5	0	0	5	0	1	0	0	1	6
04:15 PM	0	0	0	0	0	0	0	1	0	1	0	2	0	0	2	0	2	1	0	3	6
04:30 PM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	1	0	1	3
*** BREAK ***																					
Total	0	0	0	0	0	0	0	1	0	1	0	9	0	0	9	0	3	2	0	5	15
05:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
05:15 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
*** BREAK ***																					
05:45 PM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	1	0	0	1	2
Total	0	0	0	0	0	0	0	1	0	1	0	1	0	0	1	0	2	0	0	2	4
Grand Total	0	0	0	0	0	0	0	2	0	2	0	10	0	0	10	0	5	2	0	7	19
Apprch %	0	0	0	0		0	0	100	0		0	100	0	0		0	71.4	28.6	0		
Total %	0	0	0	0		0	0	10.5	0	10.5	0	52.6	0	0	52.6	0	26.3	10.5	0	36.8	

Start Time	SAXON BLVD Northbound					SAXON BLVD Southbound					DOYLE RD Eastbound					DOYLE RD Westbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:00 PM																					
04:00 PM	0	0	0	0	0	0	0	0	0	0	0	5	0	0	5	0	1	0	0	1	6
04:15 PM	0	0	0	0	0	0	0	1	0	1	0	2	0	0	2	0	2	1	0	3	6
04:30 PM	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	1	0	1	3
04:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	1	0	1	0	9	0	0	9	0	3	2	0	5	15
% App. Total	0	0	0	0		0	0	100	0		0	100	0	0		0	60	40	0		
PHF	.000	.000	.000	.000	.000	.000	.000	.250	.000	.250	.000	.450	.000	.000	.450	.000	.375	.500	.000	.417	.625

Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1

Peak Hour for Each Approach Begins at:

	04:00 PM					04:00 PM					04:00 PM					04:00 PM				
+0 mins.	0	0	0	0	0	0	0	0	0	0	0	5	0	0	5	0	1	0	0	1
+15 mins.	0	0	0	0	0	0	0	1	0	1	0	2	0	0	2	0	2	1	0	3
+30 mins.	0	0	0	0	0	0	0	0	0	0	0	2	0	0	2	0	0	1	0	1
+45 mins.	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Total Volume	0	0	0	0	0	0	0	1	0	1	0	9	0	0	9	0	3	2	0	5
% App. Total	0	0	0	0		0	0	100	0		0	100	0	0		0	60	40	0	
PHF	.00	.00	.00	.00	.000	.00	.00	.25	.00	.250	.00	.45	.00	.00	.450	.00	.37	.50	.00	.417

Groups Printed- All Vehicles

Start Time	COURTLAND BLVD Northbound					COURTLAND BLVD Southbound					DOYLE RD Eastbound					DOYLE RD Westbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
04:00 PM	8	9	5	0	22	5	9	33	0	47	38	36	15	0	89	8	51	18	0	77	235
04:15 PM	8	7	0	1	16	5	12	23	2	42	44	48	9	0	101	1	48	22	0	71	230
04:30 PM	4	8	2	0	14	13	10	19	0	42	45	50	3	0	98	4	76	19	0	99	253
04:45 PM	14	15	0	2	31	10	8	20	0	38	53	34	7	0	94	7	54	26	2	89	252
Total	34	39	7	3	83	33	39	95	2	169	180	168	34	0	382	20	229	85	2	336	970
05:00 PM	8	12	4	0	24	7	15	13	0	35	62	34	6	0	102	3	59	38	0	100	261
05:15 PM	12	18	5	0	35	16	5	25	0	46	68	32	14	0	114	7	56	32	0	95	290
05:30 PM	10	16	1	3	30	9	10	32	2	53	67	26	12	0	105	6	50	31	0	87	275
05:45 PM	14	14	6	0	34	9	13	23	0	45	53	29	13	0	95	9	57	23	0	89	263
Total	44	60	16	3	123	41	43	93	2	179	250	121	45	0	416	25	222	124	0	371	1089
Grand Total	78	99	23	6	206	74	82	188	4	348	430	289	79	0	798	45	451	209	2	707	2059
Apprch %	37.9	48.1	11.2	2.9		21.3	23.6	54	1.1		53.9	36.2	9.9	0		6.4	63.8	29.6	0.3		
Total %	3.8	4.8	1.1	0.3	10	3.6	4	9.1	0.2	16.9	20.9	14	3.8	0	38.8	2.2	21.9	10.2	0.1	34.3	

Start Time	COURTLAND BLVD Northbound					COURTLAND BLVD Southbound					DOYLE RD Eastbound					DOYLE RD Westbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 05:00 PM																					
05:00 PM	8	12	4	0	24	7	15	13	0	35	62	34	6	0	102	3	59	38	0	100	261
05:15 PM	12	18	5	0	35	16	5	25	0	46	68	32	14	0	114	7	56	32	0	95	290
05:30 PM	10	16	1	3	30	9	10	32	2	53	67	26	12	0	105	6	50	31	0	87	275
05:45 PM	14	14	6	0	34	9	13	23	0	45	53	29	13	0	95	9	57	23	0	89	263
Total Volume	44	60	16	3	123	41	43	93	2	179	250	121	45	0	416	25	222	124	0	371	1089
% App. Total	35.8	48.8	13	2.4		22.9	24	52	1.1		60.1	29.1	10.8	0		6.7	59.8	33.4	0		
PHF	.786	.833	.667	.250	.879	.641	.717	.727	.250	.844	.919	.890	.804	.000	.912	.694	.941	.816	.000	.928	.939

Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1

Peak Hour for Each Approach Begins at:

	05:00 PM					05:00 PM					05:00 PM					04:30 PM				
+0 mins.	8	12	4	0	24	7	15	13	0	35	62	34	6	0	102	4	76	19	0	99
+15 mins.	12	18	5	0	35	16	5	25	0	46	68	32	14	0	114	7	54	26	2	89
+30 mins.	10	16	1	3	30	9	10	32	2	53	67	26	12	0	105	3	59	38	0	100
+45 mins.	14	14	6	0	34	9	13	23	0	45	53	29	13	0	95	7	56	32	0	95
Total Volume	44	60	16	3	123	41	43	93	2	179	250	121	45	0	416	21	245	115	2	383
% App. Total	35.8	48.8	13	2.4		22.9	24	52	1.1		60.1	29.1	10.8	0		5.5	64	30	0.5	
PHF	.786	.833	.667	.250	.879	.641	.717	.727	.250	.844	.919	.890	.804	.000	.912	.750	.800	.750	.250	.958
	6	3	7	0		1	7	7	0		9	0	4	0		0	6	7	0	

Groups Printed- Heavy Trucks

Start Time	COURTLAND BLVD Northbound					COURTLAND BLVD Southbound					DOYLE RD Eastbound					DOYLE RD Westbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
04:00 PM	0	0	0	0	0	0	2	1	0	3	0	4	0	0	4	0	2	0	0	2	9
04:15 PM	0	0	0	1	1	0	0	1	2	3	0	2	0	0	2	0	0	0	0	0	6
04:30 PM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	1	0	0	1	2
04:45 PM	0	0	0	2	2	0	0	0	0	0	0	1	0	0	1	0	0	0	2	2	5
Total	0	0	0	3	3	0	2	2	2	6	1	7	0	0	8	0	3	0	2	5	22
05:00 PM	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1
05:15 PM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1
05:30 PM	0	0	0	3	3	0	0	0	1	1	0	0	0	0	0	0	1	0	0	1	5
05:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1
Total	0	0	0	3	3	0	0	1	1	2	0	1	0	0	1	0	2	0	0	2	8
Grand Total	0	0	0	6	6	0	2	3	3	8	1	8	0	0	9	0	5	0	2	7	30
Apprch %	0	0	0	100		0	25	37.5	37.5		11.1	88.9	0	0		0	71.4	0	28.6		
Total %	0	0	0	20	20	0	6.7	10	10	26.7	3.3	26.7	0	0	30	0	16.7	0	6.7	23.3	

Start Time	COURTLAND BLVD Northbound					COURTLAND BLVD Southbound					DOYLE RD Eastbound					DOYLE RD Westbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
04:00 PM	0	0	0	0	0	0	2	1	0	3	0	4	0	0	4	0	2	0	0	2	9
04:15 PM	0	0	0	1	1	0	0	1	2	3	0	2	0	0	2	0	0	0	0	0	6
04:30 PM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	1	0	0	1	2
04:45 PM	0	0	0	2	2	0	0	0	0	0	0	1	0	0	1	0	0	0	2	2	5
Total Volume	0	0	0	3	3	0	2	2	2	6	1	7	0	0	8	0	3	0	2	5	22
% App. Total	0	0	0	100		0	33.3	33.3	33.3		12.5	87.5	0	0		0	60	0	40		
PHF	.000	.000	.000	.375	.375	.000	.250	.500	.250	.500	.250	.438	.000	.000	.500	.000	.375	.000	.250	.625	.611

Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1

Peak Hour for Entire Intersection Begins at 04:00 PM

+0 mins.	04:45 PM					04:00 PM					04:00 PM					04:00 PM					
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
+0 mins.	0	0	0	2	2	0	2	1	0	3	0	4	0	0	4	0	2	0	0	2	9
+15 mins.	0	0	0	0	0	0	0	1	2	3	0	2	0	0	2	0	0	0	0	0	6
+30 mins.	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	1	0	0	1	2
+45 mins.	0	0	0	3	3	0	0	0	0	0	0	1	0	0	1	0	0	0	2	2	5
Total Volume	0	0	0	5	5	0	2	2	2	6	1	7	0	0	8	0	3	0	2	5	22
% App. Total	0	0	0	100		0	33.3	33.3	33.3		12.5	87.5	0	0		0	60	0	40		
PHF	.00	.00	.00	.41	.417	.00	.25	.50	.25	.500	.25	.43	.00	.00	.500	.00	.37	.00	.25	.625	
	0	0	0	7		0	0	0	0		0	8	0	0		0	5	0	0		

Groups Printed- All Vehicles

Start Time	SR 415 Northbound					SR 415 Southbound					DOYLE RD Eastbound					DOYLE RD Westbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
04:00 PM	49	138	0	0	187	2	97	18	0	117	29	0	34	0	63	0	0	0	0	0	367
04:15 PM	49	151	0	0	200	2	99	23	0	124	24	0	37	0	61	0	0	0	0	0	385
04:30 PM	63	182	0	0	245	0	108	30	0	138	20	0	46	0	66	0	0	0	0	0	449
04:45 PM	62	174	0	0	236	1	83	23	0	107	19	0	29	0	48	0	0	0	0	0	391
Total	223	645	0	0	868	5	387	94	0	486	92	0	146	0	238	0	0	0	0	0	1592
05:00 PM	79	184	0	0	263	1	113	15	0	129	23	0	29	0	52	0	0	0	0	0	444
05:15 PM	69	198	0	0	267	4	109	18	0	131	19	0	31	0	50	0	0	0	0	0	448
05:30 PM	66	200	0	0	266	2	116	17	0	135	10	0	34	1	45	0	0	0	0	0	446
05:45 PM	82	223	0	0	305	2	79	17	0	98	14	0	27	0	41	0	0	0	0	0	444
Total	296	805	0	0	1101	9	417	67	0	493	66	0	121	1	188	0	0	0	0	0	1782
Grand Total	519	1450	0	0	1969	14	804	161	0	979	158	0	267	1	426	0	0	0	0	0	3374
Apprch %	26.4	73.6	0	0		1.4	82.1	16.4	0		37.1	0	62.7	0.2		0	0	0	0	0	
Total %	15.4	43	0	0	58.4	0.4	23.8	4.8	0	29	4.7	0	7.9	0	12.6	0	0	0	0	0	

Start Time	SR 415 Northbound					SR 415 Southbound					DOYLE RD Eastbound					DOYLE RD Westbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 05:00 PM																					
05:00 PM	79	184	0	0	263	1	113	15	0	129	23	0	29	0	52	0	0	0	0	0	444
05:15 PM	69	198	0	0	267	4	109	18	0	131	19	0	31	0	50	0	0	0	0	0	448
05:30 PM	66	200	0	0	266	2	116	17	0	135	10	0	34	1	45	0	0	0	0	0	446
05:45 PM	82	223	0	0	305	2	79	17	0	98	14	0	27	0	41	0	0	0	0	0	444
Total Volume	296	805	0	0	1101	9	417	67	0	493	66	0	121	1	188	0	0	0	0	0	1782
% App. Total	26.9	73.1	0	0		1.8	84.6	13.6	0		35.1	0	64.4	0.5		0	0	0	0	0	
PHF	.902	.902	.000	.000	.902	.563	.899	.931	.000	.913	.717	.000	.890	.250	.904	.000	.000	.000	.000	.000	.994

Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1

Peak Hour for Each Approach Begins at:

	05:00 PM					04:30 PM					04:00 PM					04:00 PM					
+0 mins.	79	184	0	0	263	0	108	30	0	138	29	0	34	0	63	0	0	0	0	0	0
+15 mins.	69	198	0	0	267	1	83	23	0	107	24	0	37	0	61	0	0	0	0	0	0
+30 mins.	66	200	0	0	266	1	113	15	0	129	20	0	46	0	66	0	0	0	0	0	0
+45 mins.	82	223	0	0	305	4	109	18	0	131	19	0	29	0	48	0	0	0	0	0	0
Total Volume	296	805	0	0	1101	6	413	86	0	505	92	0	146	0	238	0	0	0	0	0	0
% App. Total	26.	73.	0	0		1.2	81.	17	0		38.	0	61.	0		0	0	0	0	0	
	9	1					8				7		3								
PHF	.90	.90	.00	.00	.902	.37	.91	.71	.00	.915	.79	.00	.79	.00	.902	.00	.00	.00	.00	.00	.000
	2	2	0	0		5	4	7	0		3	0	3	0		0	0	0	0	0	

Groups Printed- Heavy Trucks

Start Time	SR 415 Northbound					SR 415 Southbound					DOYLE RD Eastbound					DOYLE RD Westbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
04:00 PM	0	3	0	0	3	0	1	1	0	2	1	0	0	0	1	0	0	0	0	0	6
04:15 PM	1	4	0	0	5	0	2	0	0	2	2	0	2	0	4	0	0	0	0	0	11
04:30 PM	1	5	0	0	6	0	1	0	0	1	0	0	1	0	1	0	0	0	0	0	8
04:45 PM	0	1	0	0	1	0	4	0	0	4	0	0	0	0	0	0	0	0	0	0	5
Total	2	13	0	0	15	0	8	1	0	9	3	0	3	0	6	0	0	0	0	0	30
05:00 PM	1	0	0	0	1	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	2
05:15 PM	0	8	0	0	8	0	3	0	0	3	1	0	1	0	2	0	0	0	0	0	13
05:30 PM	1	1	0	0	2	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0	5
05:45 PM	0	3	0	0	3	2	0	0	0	2	0	0	0	0	0	0	0	0	0	0	5
Total	2	12	0	0	14	2	7	0	0	9	1	0	1	0	2	0	0	0	0	0	25
Grand Total	4	25	0	0	29	2	15	1	0	18	4	0	4	0	8	0	0	0	0	0	55
Apprch %	13.8	86.2	0	0		11.1	83.3	5.6	0		50	0	50	0		0	0	0	0		
Total %	7.3	45.5	0	0	52.7	3.6	27.3	1.8	0	32.7	7.3	0	7.3	0	14.5	0	0	0	0	0	

Start Time	SR 415 Northbound					SR 415 Southbound					DOYLE RD Eastbound					DOYLE RD Westbound					Int. Total
	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	Left	Thru	Right	Peds	App. Total	
Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1																					
Peak Hour for Entire Intersection Begins at 04:00 PM																					
04:00 PM	0	3	0	0	3	0	1	1	0	2	1	0	0	0	1	0	0	0	0	0	6
04:15 PM	1	4	0	0	5	0	2	0	0	2	2	0	2	0	4	0	0	0	0	0	11
04:30 PM	1	5	0	0	6	0	1	0	0	1	0	0	1	0	1	0	0	0	0	0	8
04:45 PM	0	1	0	0	1	0	4	0	0	4	0	0	0	0	0	0	0	0	0	0	5
Total Volume	2	13	0	0	15	0	8	1	0	9	3	0	3	0	6	0	0	0	0	0	30
% App. Total	13.3	86.7	0	0		0	88.9	11.1	0		50	0	50	0		0	0	0	0		
PHF	.500	.650	.000	.000	.625	.000	.500	.250	.000	.563	.375	.000	.375	.000	.375	.000	.000	.000	.000	.000	.682

Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1

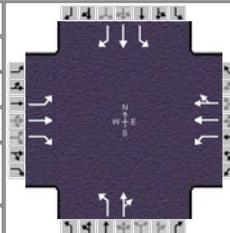
Peak Hour for Each Approach Begins at:

	04:30 PM					04:45 PM					04:00 PM					04:00 PM				
+0 mins.	1	5	0	0	6	0	4	0	0	4	1	0	0	0	1	0	0	0	0	0
+15 mins.	0	1	0	0	1	0	1	0	0	1	2	0	2	0	4	0	0	0	0	0
+30 mins.	1	0	0	0	1	0	3	0	0	3	0	0	1	0	1	0	0	0	0	0
+45 mins.	0	8	0	0	8	0	3	0	0	3	0	0	0	0	0	0	0	0	0	0
Total Volume	2	14	0	0	16	0	11	0	0	11	3	0	3	0	6	0	0	0	0	0
% App. Total	12.5	87.5	0	0		0	100	0	0		50	0	50	0		0	0	0	0	
PHF	.50	.43	.00	.00	.500	.00	.68	.00	.00	.688	.37	.00	.37	.00	.375	.00	.00	.00	.00	.000
	0	8	0	0		0	8	0	0		5	0	5	0		0	0	0	0	

EXISTING CONDITIONS HCS PRINTOUTS

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information	
Agency		Analysis Date	Nov 2, 2015	Duration, h	1.00
Analyst		Time Period	5pm-6pm	Area Type	Other
Jurisdiction		Analysis Year	2015 Existing Condition	PHF	1.00
Intersection	Providence Blvd			Analysis Period	1 > 7:00
File Name	Doyle at Providence Existing.xus				
Project Description	Existing Condition PM Peak Hour				



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	638	827	243	23	253	74	102	221	40	47	115	226

Signal Information				Signal Timing (s)									Signal Phases											
Cycle, s	91.5	Reference Phase	2	Green	2.2	5.8	27.2	3.5	2.3	15.6	Yellow	4.5	4.5	4.5	4.0	0.0	4.0	Red	2.5	2.5	2.5	3.0	0.0	3.0
Offset, s	0	Reference Point	Begin																					
Uncoordinated	Yes	Simult. Gap E/W	On																					
Force Mode	Fixed	Simult. Gap N/S	On																					

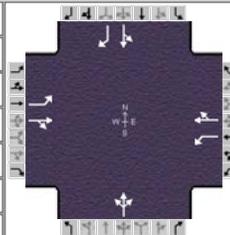
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	1.1	4.0	1.1	3.0
Phase Duration, s	22.0	47.0	9.2	34.2	12.8	24.8	10.5	22.6
Change Period, (Y+R _c), s	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0
Max Allow Headway (MAH), s	3.0	3.0	3.0	3.0	3.2	3.2	3.1	3.2
Queue Clearance Time (g _s), s	17.0	42.0	2.8	8.6	6.1	14.4	4.0	14.5
Green Extension Time (g _e), s	0.0	0.0	0.0	2.9	0.1	1.0	0.1	1.0
Phase Call Probability	1.00	1.00	0.44	1.00	0.93	1.00	0.70	1.00
Max Out Probability	1.00	1.00	0.00	0.00	0.00	0.02	0.00	0.03

Movement Group Results	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	638	827	243	23	168	159	102	261		47	115	226
Adjusted Saturation Flow Rate (s), veh/h/ln	1810	1881	1586	1810	1900	1710	1774	1809		1774	1863	1596
Queue Service Time (g _s), s	15.0	40.0	9.3	0.8	6.3	6.6	4.1	12.4		2.0	5.0	12.5
Cycle Queue Clearance Time (g _c), s	15.0	40.0	9.3	0.8	6.3	6.6	4.1	12.4		2.0	5.0	12.5
Capacity (c), veh/h	616	822	693	134	565	508	310	353		212	317	271
Volume-to-Capacity Ratio (X)	1.035	1.006	0.351	0.171	0.298	0.312	0.329	0.740		0.221	0.363	0.833
Available Capacity (c _a), veh/h	616	822	693	387	830	747	489	494		629	509	436
Back of Queue (Q), veh/ln (50th percentile)	24.9	31.4	3.1	0.3	2.7	2.5	1.7	5.6		0.8	2.2	5.0
Overflow Queue (Q ₃), veh/ln	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0
Queue Storage Ratio (RQ) (50th percentile)	1.04	0.47	0.39	0.07	0.04	0.04	0.44	0.09		0.21	0.04	1.25
Uniform Delay (d ₁), s/veh	17.9	25.8	17.1	25.0	24.8	24.9	27.1	34.7		30.1	33.6	36.7
Incremental Delay (d ₂), s/veh	112.1	68.8	0.1	0.2	0.1	0.1	0.2	1.9		0.2	0.3	3.7
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0
Control Delay (d), s/veh	130.0	94.6	17.3	25.2	24.9	25.0	27.3	36.6		30.3	33.9	40.5
Level of Service (LOS)	F	F	B	C	C	C	C	D		C	C	D
Approach Delay, s/veh / LOS	96.8		F	25.0		C	34.0		C	37.3		D
Intersection Delay, s/veh / LOS	71.5						E					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	2.3	B	2.4	B	2.4	B	2.8	C
Bicycle LOS Score / LOS	3.3	C	0.8	A	1.1	A	1.1	A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information		
Agency		Duration, h	1.00			
Analyst		Analysis Date	Nov 2, 2015		Area Type	Other
Jurisdiction		Time Period	5pm-6pm		PHF	1.00
Intersection	Saxon Blvd	Analysis Year	2015		Analysis Period	1 > 7:00
File Name	Doyle at Saxon Existing.xus					
Project Description	Existing PM Peak Hour					



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	212	388	1	2	190	118	0	0	1	55	1	104

Signal Information														
Cycle, s	48.4	Reference Phase	2											
Offset, s	0	Reference Point	End											
Uncoordinated	Yes	Simult. Gap E/W	On											
Force Mode	Fixed	Simult. Gap N/S	On											
				Green	5.2	17.0	6.2	0.0	0.0	0.0				
				Yellow	4.5	4.5	4.5	0.0	0.0	0.0				
				Red	2.0	2.0	2.5	0.0	0.0	0.0				

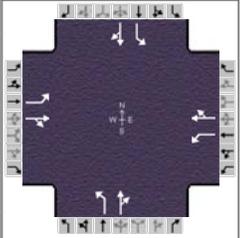
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2		6		8		4
Case Number	1.0	4.0		6.3		8.0		7.0
Phase Duration, s	11.7	35.2		23.5		13.2		13.2
Change Period, (Y+R _c), s	6.5	6.5		6.5		7.0		7.0
Max Allow Headway (MAH), s	3.1	3.0		3.0		3.2		3.2
Queue Clearance Time (g _s), s	5.2	7.1		8.7		2.0		5.0
Green Extension Time (g _e), s	0.3	1.3		1.3		0.3		0.2
Phase Call Probability	0.94	1.00		1.00		0.88		0.88
Max Out Probability	0.00	0.00		0.00		0.00		0.00

Movement Group Results	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	212	389		2	308			0			56	104
Adjusted Saturation Flow Rate (s), veh/h/ln	1810	1880		1010	1760			0			1448	1579
Queue Service Time (g _s), s	3.2	5.1		0.1	6.7			0.0			1.7	3.0
Cycle Queue Clearance Time (g _c), s	3.2	5.1		0.1	6.7			0.0			1.7	3.0
Capacity (c), veh/h	575	1114		504	619						333	202
Volume-to-Capacity Ratio (X)	0.369	0.349		0.004	0.498			0.000			0.168	0.514
Available Capacity (c _a), veh/h	943	1750		1089	1638						746	653
Back of Queue (Q), veh/ln (50th percentile)	0.8	1.1		0.0	2.0						0.5	1.0
Overflow Queue (Q ₃), veh/ln	0.0	0.0		0.0	0.0			0.0			0.0	0.0
Queue Storage Ratio (RQ) (50th percentile)	0.06	0.02		0.00	0.03			0.00			0.01	0.06
Uniform Delay (d ₁), s/veh	7.8	5.1		10.2	12.3						19.1	19.7
Incremental Delay (d ₂), s/veh	0.1	0.1		0.0	0.2			0.0			0.1	0.8
Initial Queue Delay (d ₃), s/veh	0.0	0.0		0.0	0.0			0.0			0.0	0.0
Control Delay (d), s/veh	8.0	5.1		10.2	12.6						19.2	20.4
Level of Service (LOS)	A	A		B	B						B	C
Approach Delay, s/veh / LOS	6.1		A	12.5		B	18.4		B	20.0		B
Intersection Delay, s/veh / LOS	10.1						B					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	2.0	B	2.3	B	2.3	B	2.3	B
Bicycle LOS Score / LOS	1.5	A	1.0	A	0.5	A	0.8	A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information	
Agency		Duration, h	1.00		
Analyst		Analysis Date	Nov 2, 2015	Area Type	Other
Jurisdiction		Time Period	5pm-6pm	PHF	1.00
Intersection	Courtland Blvd	Analysis Year	2015	Analysis Period	1 > 7:00
File Name	Doyle at Courtland Existing.xus				
Project Description	PM Peak Hour Existing				



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	250	121	45	25	222	124	44	60	16	41	43	93

Signal Information				Signal Phases							
Cycle, s	46.6	Reference Phase	2								
Offset, s	0	Reference Point	End	Green	6.3	12.0	7.3	0.0	0.0	0.0	
Uncoordinated	Yes	Simult. Gap E/W	On	Yellow	4.5	4.5	4.0	0.0	0.0	0.0	
Force Mode	Fixed	Simult. Gap N/S	On	Red	2.5	2.5	3.0	0.0	0.0	0.0	

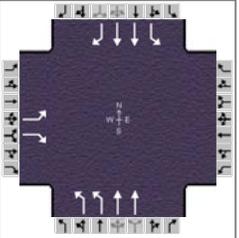
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	1	6		2		4		8
Case Number	1.0	4.0		6.3		6.0		6.0
Phase Duration, s	13.3	32.3		19.0		14.3		14.3
Change Period, (Y+R _c), s	7.0	7.0		7.0		7.0		7.0
Max Allow Headway (MAH), s	3.1	3.1		3.1		3.2		3.2
Queue Clearance Time (g _s), s	6.2	4.2		10.4		7.0		5.4
Green Extension Time (g _e), s	0.3	1.0		1.0		0.5		0.5
Phase Call Probability	0.96	1.00		1.00		0.98		0.98
Max Out Probability	0.01	0.00		0.00		0.00		0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	1	6	16	5	2	12	7	4	14	3	8	18
Adjusted Flow Rate (v), veh/h	250	166		25	346		44	76		41	136	
Adjusted Saturation Flow Rate (s), veh/h/ln	1810	1794		1239	1767		1273	1831		1344	1692	
Queue Service Time (g _s), s	4.2	2.2		0.7	8.4		1.5	1.7		1.3	3.4	
Cycle Queue Clearance Time (g _c), s	4.2	2.2		0.7	8.4		5.0	1.7		3.0	3.4	
Capacity (c), veh/h	479	972		473	455		261	289		317	267	
Volume-to-Capacity Ratio (X)	0.522	0.171		0.053	0.760		0.169	0.263		0.129	0.510	
Available Capacity (c _a), veh/h	817	1154		952	1137		743	982		826	907	
Back of Queue (Q), veh/ln (50th percentile)	1.2	0.5		0.2	2.8		0.4	0.6		0.3	1.2	
Overflow Queue (Q ₃), veh/ln	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Queue Storage Ratio (RQ) (50th percentile)	0.00	0.00		0.00	0.00		0.00	0.00		0.00	0.00	
Uniform Delay (d ₁), s/veh	10.2	5.4		13.1	16.0		20.3	17.3		18.6	18.0	
Incremental Delay (d ₂), s/veh	0.3	0.0		0.0	1.0		0.1	0.2		0.1	0.6	
Initial Queue Delay (d ₃), s/veh	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Control Delay (d), s/veh	10.5	5.4		13.1	17.0		20.4	17.4		18.7	18.6	
Level of Service (LOS)	B	A		B	B		C	B		B	B	
Approach Delay, s/veh / LOS	8.5	A		16.7	B		18.5	B		18.6	B	
Intersection Delay, s/veh / LOS	14.1						B					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	2.2	B	2.3	B	2.3	B	2.3	B
Bicycle LOS Score / LOS	1.2	A	1.1	A	0.7	A	0.8	A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information			
Agency		Duration, h	1.00				
Analyst		Analysis Date	Nov 2, 2015		Area Type	Other	
Jurisdiction		Time Period	5pm-6pm		PHF	1.00	
Intersection	Doyle Rd		Analysis Year	2015		Analysis Period	1 > 7:00
File Name	SR 415 at Doyle Existing.xus						
Project Description	Existing PM Peak Hour						



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	66		121				296	805		9	417	67

Signal Information				Phase Timings (s)						Phase Diagrams				
Cycle, s	90.0	Reference Phase	2	Green	1.0	2.4	53.3	8.8	0.0	0.0	1	2	3	4
Offset, s	0	Reference Point	End	Yellow	4.5	4.5	4.5	4.0	0.0	0.0	5	6	7	8
Uncoordinated	No	Simult. Gap E/W	On	Red	2.0	2.0	2.0	1.0	0.0	0.0				
Force Mode	Fixed	Simult. Gap N/S	On											

Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase		8			1	6	5	2
Case Number		9.0			2.0	4.0	2.0	3.0
Phase Duration, s		13.8			16.4	68.7	7.5	59.8
Change Period, (Y+R _c), s		5.0			6.5	6.5	6.5	6.5
Max Allow Headway (MAH), s		3.2			3.1	0.0	3.1	0.0
Queue Clearance Time (g _s), s		8.7			9.4		2.4	
Green Extension Time (g _e), s		0.2			0.5	0.0	0.0	0.0
Phase Call Probability		0.99			1.00		0.20	
Max Out Probability		0.05			0.00		0.00	

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	3		18				1	6		5	2	12
Adjusted Flow Rate (v), veh/h	66		121				296	805		9	417	67
Adjusted Saturation Flow Rate (s), veh/h/ln	1792		1594				1740	1773		1810	1773	1610
Queue Service Time (g _s), s	3.1		6.7				7.4	8.2		0.4	4.9	1.6
Cycle Queue Clearance Time (g _c), s	3.1		6.7				7.4	8.2		0.4	4.9	1.6
Capacity (c), veh/h	175		155				384	2452		20	2100	953
Volume-to-Capacity Ratio (X)	0.378		0.779				0.770	0.328		0.444	0.199	0.070
Available Capacity (c _a), veh/h	299		266				763	2452		397	2100	953
Back of Queue (Q), veh/ln (50th percentile)	1.3		2.7				3.1	2.4		0.2	1.7	0.5
Overflow Queue (Q ₃), veh/ln	0.0		0.0				0.0	0.0		0.0	0.0	0.0
Queue Storage Ratio (RQ) (50th percentile)	0.06		0.12				0.24	0.04		0.05	0.03	0.04
Uniform Delay (d ₁), s/veh	38.1		39.7				38.9	5.5		44.2	8.5	7.8
Incremental Delay (d ₂), s/veh	0.5		3.2				1.3	0.4		5.7	0.2	0.1
Initial Queue Delay (d ₃), s/veh	0.0		0.0				0.0	0.0		0.0	0.0	0.0
Control Delay (d), s/veh	38.6		42.9				40.2	5.9		49.9	8.7	8.0
Level of Service (LOS)	D		D				D	A		D	A	A
Approach Delay, s/veh / LOS	41.4		D		0.0		15.1	B		9.3		A
Intersection Delay, s/veh / LOS	16.3						B					

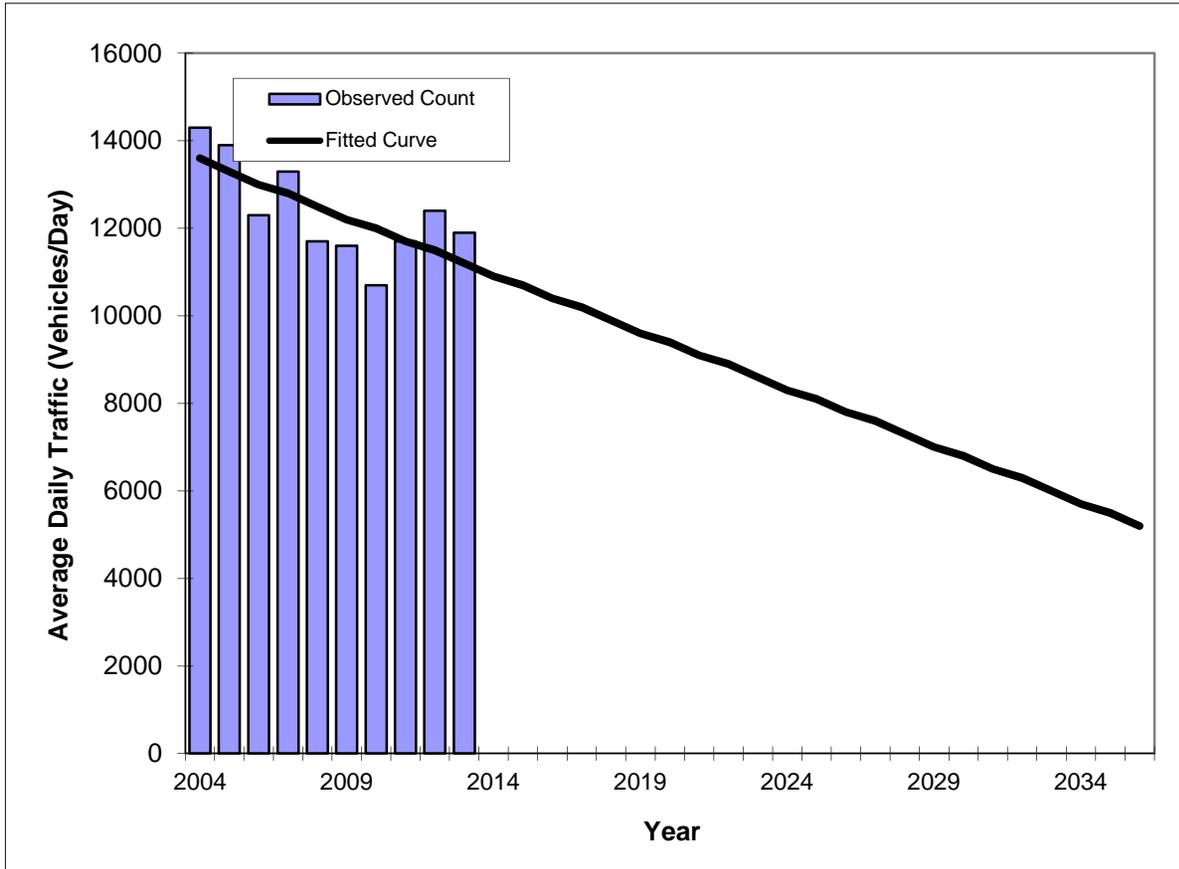
Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	3.0	C	3.0	C	1.9	A	2.7	B
Bicycle LOS Score / LOS		F			1.4	A	0.9	A

HISTORICAL TRENDS SPREADSHEETS

TRAFFIC TRENDS

Doyle Rd -- Providence Blvd

County:	Volusia
Station #:	485
Highway:	Doyle Rd



Year	Traffic (ADT/AADT)	
	Count*	Trend**
2004	14300	13600
2005	13900	13300
2006	12300	13000
2007	13300	12800
2008	11700	12500
2009	11600	12200
2010	10700	12000
2011	11700	11700
2012	12400	11500
2013	11900	11200
2014 Opening Year Trend		
2014	N/A	10900
2015 Mid-Year Trend		
2015	N/A	10700
2016 Design Year Trend		
2016	N/A	10400
TRANPLAN Forecasts/Trends		

**** Annual Trend Increase:** -261
Trend R-squared: 49.0%
Trend Annual Historic Growth Rate: -1.96%
Trend Growth Rate (2013 to Design Year): -2.38%
Printed: 11-Nov-15

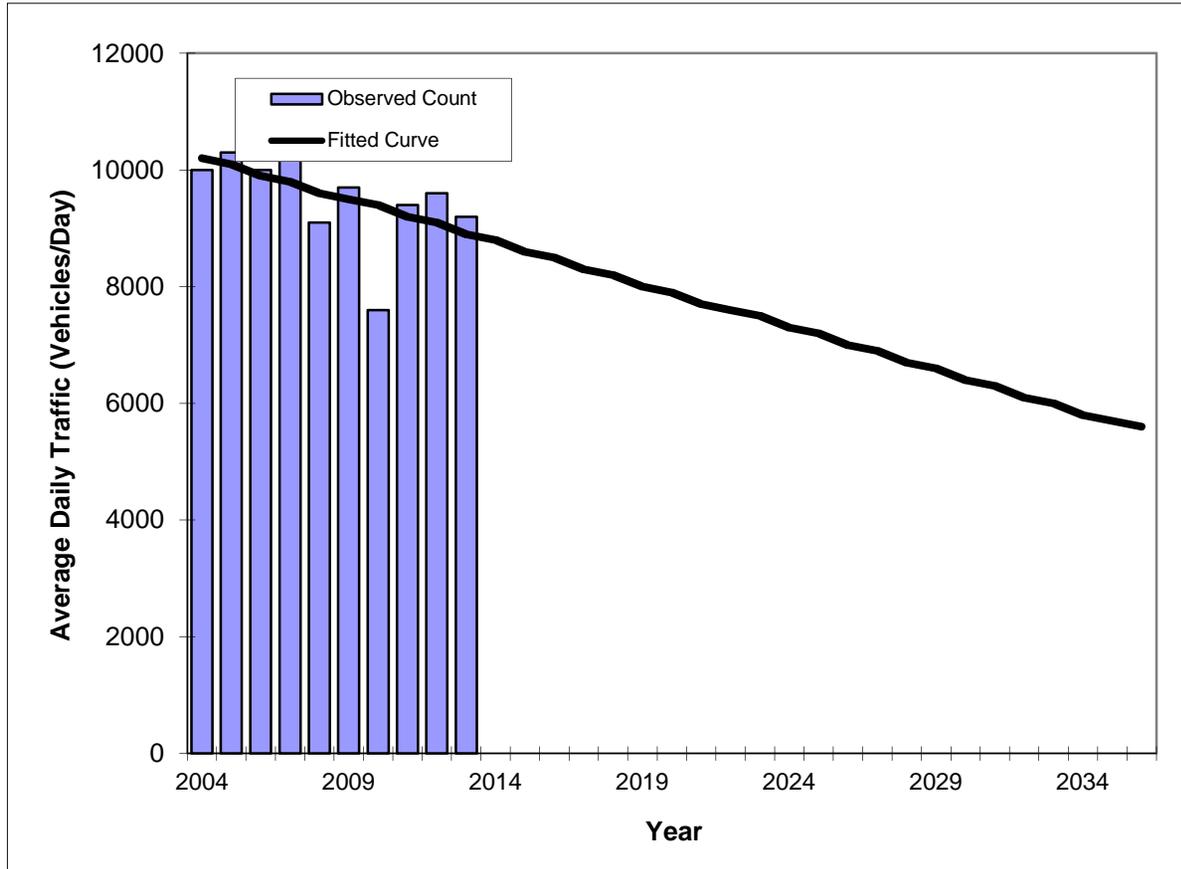
Straight Line Growth Option

*Axle-Adjusted

TRAFFIC TRENDS

Doyle Rd -- Saxon Blvd

County:	Volusia
Station #:	530
Highway:	Doyle Rd



Year	Traffic (ADT/AADT)	
	Count*	Trend**
2004	10000	10200
2005	10300	10100
2006	10000	9900
2007	10800	9800
2008	9100	9600
2009	9700	9500
2010	7600	9400
2011	9400	9200
2012	9600	9100
2013	9200	8900
2014 Opening Year Trend		
2014	N/A	8800
2015 Mid-Year Trend		
2015	N/A	8600
2016 Design Year Trend		
2016	N/A	8500
TRANPLAN Forecasts/Trends		

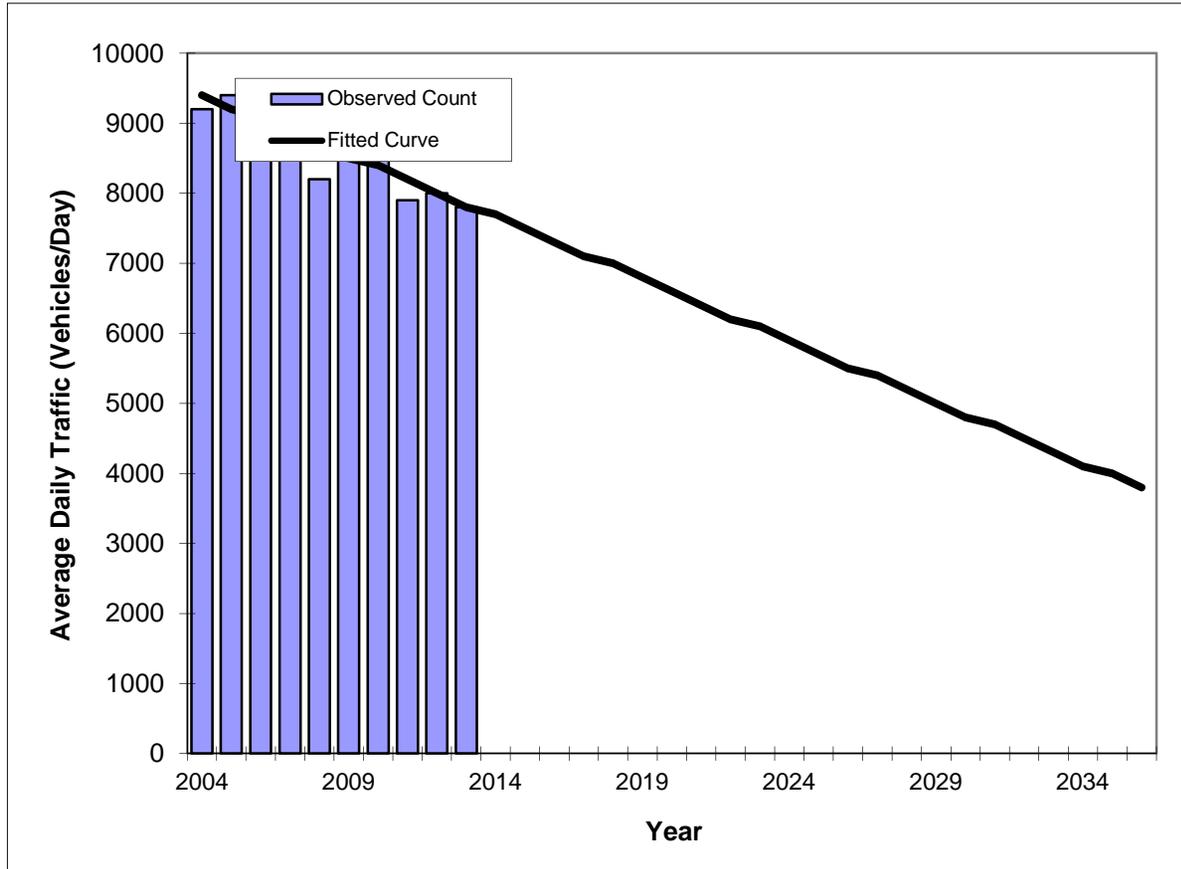
** Annual Trend Increase: -146
 Trend R-squared: 26.3%
 Trend Annual Historic Growth Rate: -1.42%
 Trend Growth Rate (2013 to Design Year): -1.50%
 Printed: 11-Nov-15
Straight Line Growth Option

*Axle-Adjusted

TRAFFIC TRENDS

Doyle Rd -- Courtland Blvd

County:	Volusia
Station #:	531
Highway:	Doyle Rd



Year	Traffic (ADT/AADT)	
	Count*	Trend**
2004	9200	9400
2005	9400	9200
2006	9200	9100
2007	9000	8900
2008	8200	8700
2009	8900	8500
2010	8700	8400
2011	7900	8200
2012	8000	8000
2013	7800	7800
2014 Opening Year Trend		
2014	N/A	7700
2015 Mid-Year Trend		
2015	N/A	7500
2016 Design Year Trend		
2016	N/A	7300
TRANPLAN Forecasts/Trends		

**** Annual Trend Increase:** -176
Trend R-squared: 78.7%
Trend Annual Historic Growth Rate: -1.89%
Trend Growth Rate (2013 to Design Year): -2.14%
Printed: 11-Nov-15

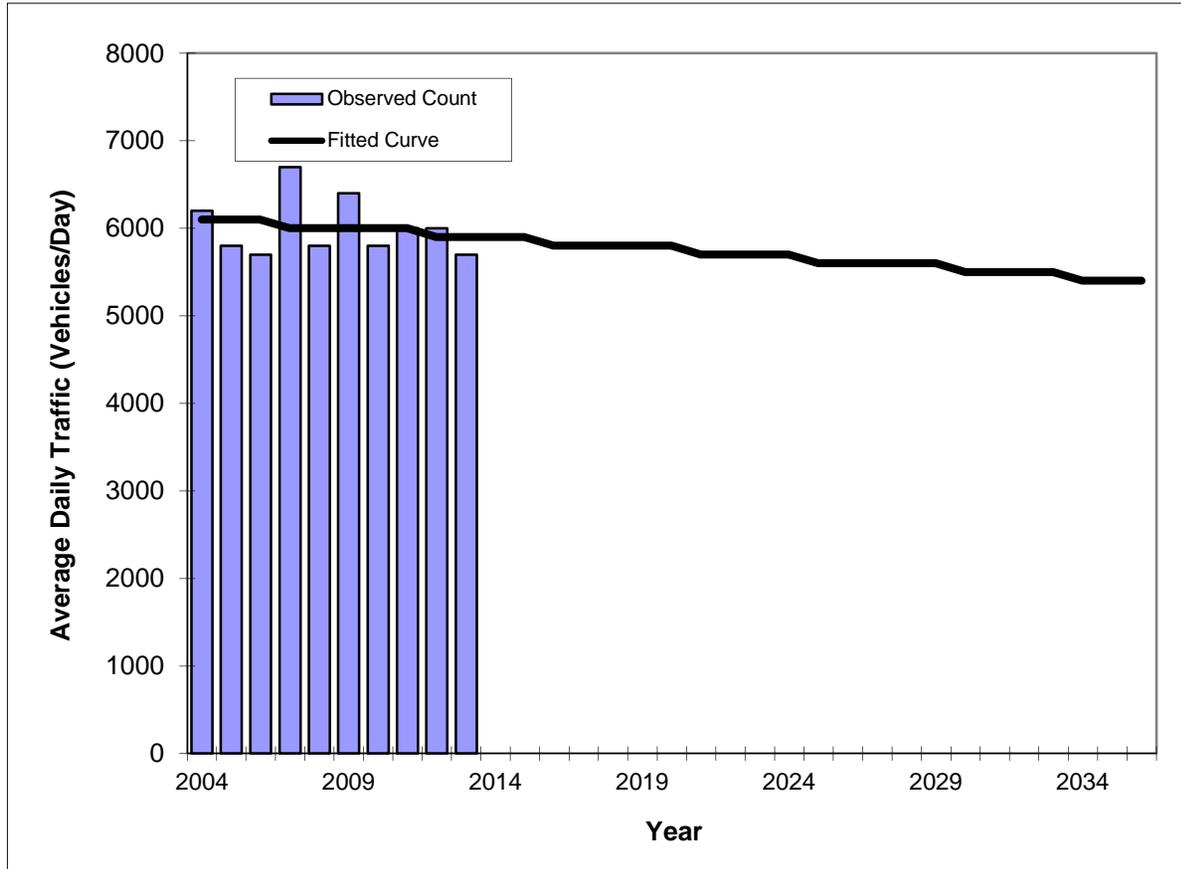
Straight Line Growth Option

*Axle-Adjusted

TRAFFIC TRENDS

Doyle Rd -- SR 415

County:	Volusia
Station #:	533
Highway:	Doyle Rd



Year	Traffic (ADT/AADT)	
	Count*	Trend**
2004	6200	6100
2005	5800	6100
2006	5700	6100
2007	6700	6000
2008	5800	6000
2009	6400	6000
2010	5800	6000
2011	6000	6000
2012	6000	5900
2013	5700	5900
2014 Opening Year Trend		
2014	N/A	5900
2015 Mid-Year Trend		
2015	N/A	5900
2016 Design Year Trend		
2016	N/A	5800
TRANPLAN Forecasts/Trends		

**** Annual Trend Increase:** -22
Trend R-squared: 4.2%
Trend Annual Historic Growth Rate: -0.36%
Trend Growth Rate (2013 to Design Year): -0.56%
Printed: 11-Nov-15

Straight Line Growth Option

*Axle-Adjusted

TURNING MOVEMENT WORKSHEETS

Turning Movement Worksheet - PM Peak Hour

State Road 415 at Doyle Road

	SR 415			SR 415			Doyle Road			Doyle Road		
	Northbound			Southbound			Eastbound			Westbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Existing												
Existing Volume	296	805	0	9	417	67	66	0	121	0	0	0
Count year	2015	2015	2015	2015	2015	2015	2015	2015	2015	2015	2015	2015
Pk Season Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adjust Exist Volume	296	805	0	9	417	67	66	0	121	0	0	0
Future Background												
Vested Trips	0	0	0	0	0	0	0	0	0	0	0	0
Growth (%)	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%
Growth (year)	2017	2017	2017	2017	2017	2017	2017	2017	2017	2017	2017	2017
Growth (volume)	6	16	0	0	8	1	1	0	2	0	0	0
Applied Growth	6	16	0	0	8	1	1	0	2	0	0	0
Volume	302	821	0	9	425	68	67	0	123	0	0	0
New External Project Trips												
% In	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
Volume In	54	0	0	0	0	86	0	0	0	0	0	0
% Out	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
Volume Out	0	0	0	0	0	0	49	0	30	0	0	0
Total Volume	54	0	0	0	0	86	49	0	30	0	0	0
Total												
Total	356	821	0	9	425	154	116	0	153	0	0	0

Turning Movement Worksheet - PM Peak Hour

Courtland Blvd at Doyle Road

	Courtland Blvd			Courtlnad Blvd			Doyle Road			Doyle Road		
	Northbound			Southbound			Eastbound			Westbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Existing												
Existing Volume	44	60	16	41	43	93	250	121	45	25	222	124
Count year	2015	2015	2015	2015	2015	2015	2015	2015	2015	2015	2015	2015
Pk Season Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adjust Exist Volume	44	60	16	41	43	93	250	121	45	25	222	124
Future Background												
Vested Trips	0	0	0	0	0	0	0	0	0	0	0	0
Growth (%)	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%
Growth (year)	2017	2017	2017	2017	2017	2017	2017	2017	2017	2017	2017	2017
Growth (volume)	1	1	0	1	1	2	5	2	1	1	4	2
Applied Growth	1	1	0	1	1	2	5	2	1	1	4	2
Volume	45	61	16	42	44	95	255	123	46	26	226	126
New External Project Trips												
% In	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
Volume In	0	0	0	11	0	0	0	65	0	0	0	0
% Out	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
Volume Out	0	0	0	0	0	0	0	0	0	0	37	6
Total Volume	0	0	0	11	0	0	0	65	0	0	37	6
Total												
Total	45	61	16	53	44	95	255	188	46	26	263	132

Turning Movement Worksheet - PM Peak Hour

Saxon Blvd at Doyle Road

	Saxon Blvd			Saxon Blvd			Doyle Road			Doyle Road		
	Northbound			Southbound			Eastbound			Westbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Existing												
Existing Volume	0	0	1	55	1	104	212	388	1	2	190	118
Count year	2015	2015	2015	2015	2015	2015	2015	2015	2015	2015	2015	2015
Pk Season Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adjust Exist Volume	0	0	1	55	1	104	212	388	1	2	190	118
Future Background												
Vested Trips	0	0	0	0	0	0	0	0	0	0	0	0
Growth (%)	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%
Growth (year)	2017	2017	2017	2017	2017	2017	2017	2017	2017	2017	2017	2017
Growth (volume)	0	0	0	1	0	2	4	8	0	0	4	2
Applied Growth	0	0	0	1	0	2	4	8	0	0	4	2
Volume	0	0	1	56	1	106	216	396	1	2	194	120
New External Project Trips												
% In	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
Volume In	0	0	0	11	0	0	0	54	0	0	0	0
% Out	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
Volume Out	0	0	0	0	0	0	0	0	0	0	31	6
Total Volume	0	0	0	11	0	0	0	54	0	0	31	6
Total												
Total	0	0	1	67	1	106	216	450	1	2	225	126

Turning Movement Worksheet - PM Peak Hour

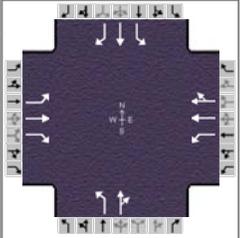
Providence Blvd at Doyle Road

	Providence Blvd			Providence blvd			Doyle Road			Doyle Road		
	Northbound			Southbound			Eastbound			Westbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Existing												
Existing Volume	102	221	40	47	115	226	638	827	243	23	253	74
Count year	2015	2015	2015	2015	2015	2015	2015	2015	2015	2015	2015	2015
Pk Season Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adjust Exist Volume	102	221	40	47	115	226	638	827	243	23	253	74
Future Background												
Vested Trips	0	0	0	0	0	0	0	0	0	0	0	0
Growth (%)	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%	1.0%
Growth (year)	2017	2017	2017	2017	2017	2017	2017	2017	2017	2017	2017	2017
Growth (volume)	2	4	1	1	2	5	13	17	5	0	5	1
Applied Growth	2	4	1	1	2	5	13	17	5	0	5	1
Volume	104	225	41	48	117	231	651	844	248	23	258	75
New External Project Trips												
% In	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
Volume In	0	0	0	4	0	0	0	50	0	0	0	0
% Out	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%
Volume Out	0	0	0	0	0	0	0	0	0	0	29	2
Total Volume	0	0	0	4	0	0	0	50	0	0	29	2
Total												
Total	104	225	41	52	117	231	651	894	248	23	287	77

FUTURE CONDITIONS HCS PRINTOUTS

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information	
Agency		Analysis Date	Nov 2, 2015	Duration, h	1.00
Analyst		Time Period	5pm-6pm	Area Type	Other
Jurisdiction		Analysis Year	2015 Existing Condition	PHF	1.00
Intersection	Providence Blvd			Analysis Period	1 > 7:00
File Name	Doyle at Providence Future.xus				
Project Description	Future Condition PM Peak Hour				



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	651	904	248	23	287	77	104	225	41	52	117	231

Signal Information														
Cycle, s	92.0	Reference Phase	2											
Offset, s	0	Reference Point	Begin											
Uncoordinated	Yes	Simult. Gap E/W	On	Green	2.2	5.8	27.2	3.7	2.2	15.9				
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.5	4.5	4.5	4.0	0.0	4.0				
				Red	2.5	2.5	2.5	3.0	0.0	3.0				

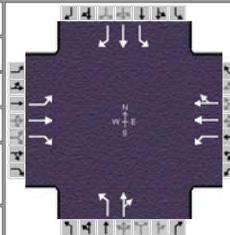
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	1.1	4.0	1.1	3.0
Phase Duration, s	22.0	47.0	9.2	34.2	12.9	25.1	10.7	22.9
Change Period, (Y+R _c), s	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0
Max Allow Headway (MAH), s	3.0	3.0	3.0	3.0	3.2	3.2	3.1	3.2
Queue Clearance Time (g _s), s	17.0	42.0	2.8	9.4	6.2	14.7	4.2	14.9
Green Extension Time (g _e), s	0.0	0.0	0.0	3.3	0.1	1.0	0.1	1.0
Phase Call Probability	1.00	1.00	0.44	1.00	0.93	1.00	0.74	1.00
Max Out Probability	1.00	1.00	0.00	0.00	0.00	0.03	0.00	0.03

Movement Group Results	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	651	904	248	23	188	176	104	266		52	117	231
Adjusted Saturation Flow Rate (s), veh/h/ln	1810	1881	1586	1810	1900	1721	1774	1809		1774	1863	1596
Queue Service Time (g _s), s	15.0	40.0	9.6	0.8	7.1	7.4	4.2	12.7		2.2	5.1	12.9
Cycle Queue Clearance Time (g _c), s	15.0	40.0	9.6	0.8	7.1	7.4	4.2	12.7		2.2	5.1	12.9
Capacity (c), veh/h	596	818	689	133	562	509	313	356		214	322	276
Volume-to-Capacity Ratio (X)	1.093	1.106	0.360	0.173	0.334	0.347	0.332	0.746		0.243	0.363	0.837
Available Capacity (c _a), veh/h	596	818	689	384	826	748	488	491		625	506	434
Back of Queue (Q), veh/ln (50th percentile)	38.0	63.7	3.2	0.3	3.0	2.9	1.8	5.7		0.9	2.3	5.2
Overflow Queue (Q ₃), veh/ln	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0
Queue Storage Ratio (RQ) (50th percentile)	1.58	0.94	0.40	0.07	0.05	0.05	0.45	0.10		0.23	0.04	1.30
Uniform Delay (d ₁), s/veh	17.8	26.0	17.4	25.2	25.3	25.4	27.0	34.8		30.0	33.6	36.8
Incremental Delay (d ₂), s/veh	196.8	211.0	0.1	0.2	0.1	0.2	0.2	2.3		0.2	0.3	4.6
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0
Control Delay (d), s/veh	214.6	237.0	17.6	25.4	25.4	25.6	27.2	37.1		30.2	33.8	41.4
Level of Service (LOS)	F	F	B	C	C	C	C	D		C	C	D
Approach Delay, s/veh / LOS	198.7	F		25.5	C		34.3	C		37.7	D	
Intersection Delay, s/veh / LOS	133.8						F					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	2.3	B	2.4	B	2.4	B	2.8	C
Bicycle LOS Score / LOS	3.5	C	0.8	A	1.1	A	1.1	A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information	
Agency		Analysis Date	Nov 2, 2015	Duration, h	1.00
Analyst		Time Period	5pm-6pm	Area Type	Other
Jurisdiction		Analysis Year	2015 Existing Condition	PHF	1.00
Intersection	Providence Blvd			Analysis Period	1 > 7:00
File Name	Doyle at Providence Future-Optimized.xus				
Project Description	Future Condition PM Peak Hour (Optimized)				



Demand Information	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Demand (v), veh/h	651	904	248	23	287	77	104	225	41	52	117	231

Signal Information													
Cycle, s	114.6	Reference Phase	2										
Offset, s	0	Reference Point	Begin										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	2.6	12.2	38.4	4.1	3.1	19.2			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.5	4.5	4.5	4.0	0.0	4.0			
				Red	2.5	2.5	2.5	3.0	0.0	3.0			

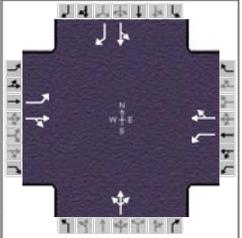
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2	1	6	3	8	7	4
Case Number	1.1	3.0	1.1	4.0	1.1	4.0	1.1	3.0
Phase Duration, s	28.8	64.6	9.6	45.4	14.1	29.3	11.1	26.2
Change Period, (Y+R _c), s	7.0	7.0	7.0	7.0	7.0	7.0	7.0	7.0
Max Allow Headway (MAH), s	3.0	3.0	3.0	3.0	3.2	3.2	3.1	3.2
Queue Clearance Time (g _s), s	21.2	54.8	2.9	10.7	7.3	17.9	4.8	18.3
Green Extension Time (g _e), s	0.6	2.7	0.0	3.3	0.1	0.9	0.1	0.9
Phase Call Probability	1.00	1.00	0.52	1.00	0.96	1.00	0.81	1.00
Max Out Probability	0.66	0.21	0.00	0.00	0.00	0.15	0.00	0.17

Movement Group Results	EB			WB			NB			SB		
Approach Movement	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	651	904	248	23	187	177	104	266		52	117	231
Adjusted Saturation Flow Rate (s), veh/h/ln	1810	1881	1589	1810	1900	1726	1774	1809		1774	1863	1586
Queue Service Time (g _s), s	19.2	52.8	10.5	0.9	8.3	8.7	5.3	15.9		2.8	6.4	16.3
Cycle Queue Clearance Time (g _c), s	19.2	52.8	10.5	0.9	8.3	8.7	5.3	15.9		2.8	6.4	16.3
Capacity (c), veh/h	677	946	800	131	639	581	316	352		188	312	266
Volume-to-Capacity Ratio (X)	0.961	0.955	0.310	0.176	0.293	0.304	0.329	0.755		0.277	0.375	0.868
Available Capacity (c _a), veh/h	728	1065	900	484	1076	978	437	394		511	406	346
Back of Queue (Q), veh/ln (50th percentile)	14.9	27.4	3.6	0.4	3.7	3.5	2.3	7.7		1.2	2.9	7.5
Overflow Queue (Q ₃), veh/ln	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0
Queue Storage Ratio (RQ) (50th percentile)	0.62	0.41	0.45	0.09	0.06	0.06	0.59	0.13		0.31	0.05	1.88
Uniform Delay (d ₁), s/veh	17.2	27.3	16.8	29.0	28.0	28.2	34.1	43.6		38.3	42.4	46.5
Incremental Delay (d ₂), s/veh	35.2	22.8	0.1	0.2	0.1	0.1	0.2	6.2		0.3	0.3	16.3
Initial Queue Delay (d ₃), s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0
Control Delay (d), s/veh	52.4	50.0	16.9	29.2	28.1	28.3	34.3	49.9		38.6	42.7	62.8
Level of Service (LOS)	D	D	B	C	C	C	C	D		D	D	E
Approach Delay, s/veh / LOS	46.3	D		28.3	C		45.5	D		53.8	D	
Intersection Delay, s/veh / LOS	44.9						D					

MultiModal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	1.7	A	1.9	A	1.9	A	1.9	A
Bicycle LOS Score / LOS	3.5	C	0.8	A	1.1	A	1.1	A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information	
Agency		Duration, h	1.00		
Analyst		Analysis Date	Nov 2, 2015	Area Type	Other
Jurisdiction		Time Period	5pm-6pm	PHF	1.00
Intersection	Saxon Blvd	Analysis Year	2015	Analysis Period	1 > 7:00
File Name	Doyle at Saxon Future.xus				
Project Description	Future PM Peak Hour				



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	216	450	1	2	225	126	0	0	1	67	1	106

Signal Information				Signal Phases									
Cycle, s	48.6	Reference Phase	2										
Offset, s	0	Reference Point	End										
Uncoordinated	Yes	Simult. Gap E/W	On	Green	5.3	17.0	6.3	0.0	0.0	0.0			
Force Mode	Fixed	Simult. Gap N/S	On	Yellow	4.5	4.5	4.5	0.0	0.0	0.0			
				Red	2.0	2.0	2.5	0.0	0.0	0.0			

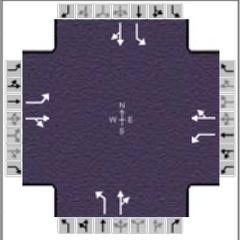
Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	5	2		6		8		4
Case Number	1.0	4.0		6.3		8.0		7.0
Phase Duration, s	11.8	35.3		23.5		13.3		13.3
Change Period, (Y+R _c), s	6.5	6.5		6.5		7.0		7.0
Max Allow Headway (MAH), s	3.1	3.0		3.0		3.2		3.2
Queue Clearance Time (g _s), s	5.3	8.3		9.8		2.0		5.0
Green Extension Time (g _e), s	0.3	1.5		1.5		0.3		0.3
Phase Call Probability	0.95	1.00		1.00		0.91		0.91
Max Out Probability	0.00	0.00		0.00		0.00		0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Assigned Movement	5	2	12	1	6	16	3	8	18	7	4	14
Adjusted Flow Rate (v), veh/h	216	451		2	351			0		68	106	
Adjusted Saturation Flow Rate (s), veh/h/ln	1810	1880		954	1767			0		1446	1579	
Queue Service Time (g _s), s	3.3	6.3		0.1	7.8			0.0		2.0	3.0	
Cycle Queue Clearance Time (g _c), s	3.3	6.3		0.1	7.8			0.0		2.1	3.0	
Capacity (c), veh/h	541	1113		482	618					336	206	
Volume-to-Capacity Ratio (X)	0.399	0.405		0.004	0.568			0.000		0.203	0.515	
Available Capacity (c _a), veh/h	903	1741		1032	1636					741	649	
Back of Queue (Q), veh/ln (50th percentile)	0.9	1.3		0.0	2.4					0.6	1.0	
Overflow Queue (Q ₃), veh/ln	0.0	0.0		0.0	0.0			0.0		0.0	0.0	
Queue Storage Ratio (RQ) (50th percentile)	0.06	0.02		0.00	0.04			0.00		0.01	0.06	
Uniform Delay (d ₁), s/veh	8.2	5.3		10.3	12.8					19.3	19.7	
Incremental Delay (d ₂), s/veh	0.2	0.1		0.0	0.3			0.0		0.1	0.7	
Initial Queue Delay (d ₃), s/veh	0.0	0.0		0.0	0.0			0.0		0.0	0.0	
Control Delay (d), s/veh	8.4	5.4		10.3	13.1					19.4	20.4	
Level of Service (LOS)	A	A		B	B					B	C	
Approach Delay, s/veh / LOS	6.4	A		13.1	B		18.4	B		20.0	C	
Intersection Delay, s/veh / LOS	10.4						B					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	2.0	B	2.3	B	2.3	B	2.3	B
Bicycle LOS Score / LOS	1.6	A	1.1	A	0.5	A	0.8	A

HCS 2010 Signalized Intersection Results Summary

General Information				Intersection Information		
Agency		Duration, h	1.00			
Analyst		Analysis Date	Nov 2, 2015		Area Type	Other
Jurisdiction		Time Period	5pm-6pm		PHF	1.00
Intersection	Courtland Blvd	Analysis Year	2015		Analysis Period	1 > 7:00
File Name	Doyle at Courtland Future.xus					
Project Description	PM Peak Hour Future					



Demand Information	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Approach Movement												
Demand (v), veh/h	255	188	46	26	263	132	45	61	16	53	44	95

Signal Information				Signal Phases								
Cycle, s	48.5	Reference Phase	2									
Offset, s	0	Reference Point	End	Green	6.5	13.3	7.7	0.0	0.0	0.0	0.0	0.0
Uncoordinated	Yes	Simult. Gap E/W	On	Yellow	4.5	4.5	4.0	0.0	0.0	0.0	0.0	0.0
Force Mode	Fixed	Simult. Gap N/S	On	Red	2.5	2.5	3.0	0.0	0.0	0.0	0.0	0.0

Timer Results	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Assigned Phase	1	6		2		4		8
Case Number	1.0	4.0		6.3		6.0		6.0
Phase Duration, s	13.5	33.8		20.3		14.7		14.7
Change Period, (Y+R _c), s	7.0	7.0		7.0		7.0		7.0
Max Allow Headway (MAH), s	3.1	3.1		3.1		3.2		3.2
Queue Clearance Time (g _s), s	6.4	5.2		12.1		7.3		5.7
Green Extension Time (g _e), s	0.3	1.2		1.2		0.5		0.6
Phase Call Probability	0.97	1.00		1.00		0.99		0.99
Max Out Probability	0.01	0.00		0.00		0.00		0.00

Movement Group Results	EB			WB			NB			SB		
	L	T	R	L	T	R	L	T	R	L	T	R
Assigned Movement	1	6	16	5	2	12	7	4	14	3	8	18
Adjusted Flow Rate (v), veh/h	255	234		26	395		45	77		53	139	
Adjusted Saturation Flow Rate (s), veh/h/ln	1810	1817		1165	1774		1270	1831		1343	1692	
Queue Service Time (g _s), s	4.4	3.2		0.8	10.1		1.6	1.8		1.7	3.7	
Cycle Queue Clearance Time (g _c), s	4.4	3.2		0.8	10.1		5.3	1.8		3.5	3.7	
Capacity (c), veh/h	457	1003		467	486		256	293		314	270	
Volume-to-Capacity Ratio (X)	0.558	0.233		0.056	0.812		0.176	0.263		0.169	0.514	
Available Capacity (c _a), veh/h	774	1123		868	1097		707	943		791	871	
Back of Queue (Q), veh/ln (50th percentile)	1.2	0.7		0.2	3.4		0.4	0.7		0.5	1.2	
Overflow Queue (Q ₃), veh/ln	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Queue Storage Ratio (RQ) (50th percentile)	0.00	0.00		0.00	0.00		0.00	0.00		0.00	0.00	
Uniform Delay (d ₁), s/veh	10.6	5.6		13.1	16.5		21.1	17.9		19.4	18.7	
Incremental Delay (d ₂), s/veh	0.4	0.0		0.0	1.3		0.1	0.2		0.1	0.6	
Initial Queue Delay (d ₃), s/veh	0.0	0.0		0.0	0.0		0.0	0.0		0.0	0.0	
Control Delay (d), s/veh	11.0	5.6		13.1	17.7		21.2	18.1		19.5	19.2	
Level of Service (LOS)	B	A		B	B		C	B		B	B	
Approach Delay, s/veh / LOS	8.4		A	17.5		B	19.2		B	19.3		B
Intersection Delay, s/veh / LOS	14.3						B					

Multimodal Results	EB		WB		NB		SB	
Pedestrian LOS Score / LOS	2.2	B	2.3	B	2.3	B	2.3	B
Bicycle LOS Score / LOS	1.3	A	1.2	A	0.7	A	0.8	A

TWO-WAY STOP CONTROL SUMMARY								
General Information				Site Information				
Analyst				Intersection	Doyle Rd at Prop Vineland Res			
Agency/Co.				Jurisdiction				
Date Performed	11/11/2015			Analysis Year	2017			
Analysis Time Period	5pm-6pm PM Peak Hour Build-Out							
Project Description								
East/West Street: Doyle Rd				North/South Street: Vineland Reserve Entry/Exit				
Intersection Orientation: East-West				Study Period (hrs): 1.00				
Vehicle Volumes and Adjustments								
Major Street	Eastbound			Westbound				
Movement	1	2	3	4	5	6		
	L	T	R	L	T	R		
Volume (veh/h)	76	187			363	140		
Peak-Hour Factor, PHF	0.95	0.95	1.00	1.00	0.95	0.95		
Hourly Flow Rate, HFR (veh/h)	80	196	0	0	382	147		
Percent Heavy Vehicles	1	--	--	0	--	--		
Median Type	Undivided							
RT Channelized			0				0	
Lanes	1	1	0	0	1	1		
Configuration	L	T			T	R		
Upstream Signal		0			0			
Minor Street	Northbound			Southbound				
Movement	7	8	9	10	11	12		
	L	T	R	L	T	R		
Volume (veh/h)				79		43		
Peak-Hour Factor, PHF	1.00	1.00	1.00	0.95	1.00	0.95		
Hourly Flow Rate, HFR (veh/h)	0	0	0	83	0	45		
Percent Heavy Vehicles	0	0	0	1	0	0		
Percent Grade (%)		0			0			
Flared Approach		N			N			
Storage		0			0			
RT Channelized			0			0		
Lanes	0	0	0	1	0	1		
Configuration				L		R		
Delay, Queue Length, and Level of Service								
Approach	Eastbound	Westbound	Northbound			Southbound		
Movement	1	4	7	8	9	10	11	12
Lane Configuration	L					L		R
v (veh/h)	80					83		45
C (m) (veh/h)	1043					357		670
v/c	0.08					0.23		0.07
95% queue length	0.25					0.90		0.22
Control Delay (s/veh)	8.7					18.1		10.8
LOS	A					C		B
Approach Delay (s/veh)	--	--				15.5		
Approach LOS	--	--				C		

TRANSPORTATION IMPACT ANALYSIS SUBMISSION CHECKLIST

	DESCRIPTION	INFORMATION INCLUDED			
		YES	NO	N/A	Remarks ¹
TRANSPORTATION IMPACT ANALYSIS REPORT DATA	4 Printed TIA Copies Signed and Sealed by Professional Engineer	✓			
	1 Electronic version of the TIA & all analysis computer files	✓			
	Site Location relative to surrounding roadway network (map)	✓			
	Description of proposed land uses	✓			
	Proposed Build-out schedule	✓			
	Study area boundaries including all Thoroughfare Road segments and intersections within appropriate radius (map)	✓			
	Existing Traffic Volumes	✓			
	Existing roadway segment analysis	✓			
	Existing intersection analysis	✓			
	List scheduled improvements within first three years of County, FDOT, and/or City Capital Improvement Programs			✓	
	Proposed development trip generation/internal capture/pass by capture	✓			
	Proposed development trip distribution and assignment (map)	✓			
	Future Background Traffic Volume Estimates	✓			
	Projected future roadway segment analysis	✓			
	Future Total Peak-Hour(s) Traffic Volume Estimates (Background + Vested + Project Trips)	✓			
	Projected future Peak-Hour(s) roadway intersection analysis including proposed turn lanes and signals	✓			
	Projected Future Roadway Concurrency Analysis	✓			
	Conclusions and Recommended Improvements	✓			
	Site access recommendations	✓			
	Concurrency mitigation strategy			✓	
APPENDIX DATA	Methodology Documentation & Conceptual Site Plan	✓			
	Traffic Count Data & Inventory of Existing Road Conditions	✓			
	Confirmation of Scheduled Improvements (Copy of Appropriate CIE)			✓	
	Existing Conditions Analysis Worksheets (HCS Printouts)	✓			
	Background Traffic Growth Worksheets	✓			
	Trip Generation, Internal Capture, Pass-By Capture Worksheets	✓			
	Future Conditions Analysis Worksheets (HCS Printouts)	✓			
	Turn Lanes Analysis Worksheets (Queue Length)		✓		
	Signal Warrant Analysis		✓	✓	
	Multi Way STOP Warrant Analysis	✓			

¹ - Remarks: Justify "NO" and "N/A"

Submitted By:  Printed Name: Chris W. West
 Date: 11-13-15

Review of Vineland Reserve Traffic Impact Analysis City of Deltona



Prepared By: BCC Engineering, Inc.
April 15, 2016

We have completed our review the Traffic Impact Analysis (TIA) prepared by Traffic Engineering Data Services (TEDS) for Vineland Reserve in the City of Deltona. The review included analysis of the existing traffic volumes and review of the proposed traffic volumes shown in the TIA prepared for the proposed improvement, review of the directional splits of the traffic, and the Level of Service on the Impacted Major Roadways with the TIA projected traffic.

Methodology: We used Accepted Engineering Practices set forth in the ITE Trip Generation Manual, 9th Edition Spreadsheet for the proposed density of each property. We compared this information and data generated to the Traffic Impact Analysis prepared by the applicant's engineering firm for accuracy and consistency. We input corresponding socio-economic data as sketch-level planning into the latest version of the Central Florida Regional Planning Model (CFRPM 5.011). From the data generated, we compared our output from the model to that shown in the prepared documents for reasonableness check. We reviewed the TIA's PM Peak Analysis using SYNCHRO including HCM 2010 Report for accuracy.

Findings:

- Only the PM Peak was included in the study, it would be beneficial if the AM Peak were also included in the study.
- The results of the Trip Generation in the study was very close to our results and considered reasonable.
- The output from the Trip Distribution did show a difference, primarily at SR 415. The model we used sent 70% of the traffic east towards SR 415 and at SR 415, 45% turned south toward Seminole County while 25% turned north toward Walmart/Daytona Beach. Other distributions did not change significantly.
- There was not a significant difference in the LOS at the intersections analyzed.

Description/ITE Code	Units	Expected Units	Total Generated Trips		
			Daily	AM Hour	PM Hour
Data Center 160	KSF ²		0	0	0
High-Cube Warehouse/Dist Center 152	KSF ²		0	0	0
Utilities 170	KSF ²		0	0	0
Utilities 170	Employees		0	0	0
Single Family Homes 210	DU	279.0	2,656	209	279
Single Family Homes 210	Acres		0	0	0
Single Family Homes 210	Persons		0	0	0
Single Family Homes 210	Vehicles		0	0	0
Apartment 220	DU		0	0	0
Apartment 220	Persons		0	0	0
Apartment 220	Vehicles		0	0	0
Low Rise Apartment 221	Occ.DU		0	0	0
High Rise Apartment 222	DU		0	0	0
Mid-Rise Apartment 223	DU		0	0	0
Rental Townhouse 224	DU		0	0	0
Resd. Condo/Townhouse 230	DU	128.0	744	56	67
Resd. Condo/Townhouse 230	Persons		0	0	0
Resd. Condo/Townhouse 230	Vehicles		0	0	0

ORDINANCE NO. 24-2016

AN ORDINANCE OF THE CITY OF DELTONA, FLORIDA, AMENDING THE OFFICIAL ZONING MAP TO REZONE APPROXIMATELY 102.7 ACRES OF LAND LOCATED BETWEEN DOYLE ROAD AND COLLINS ROAD NEAR OSTEEN ELEMENTARY SCHOOL FROM CITY OF DELTONA RESIDENTIAL PLANNED UNIT DEVELOPMENT (RPUD) TO RESIDENTIAL PLANNED UNIT DEVELOPMENT: PROVIDING FOR CONFLICTS, PROVIDING FOR SEVERABILITY AND PROVIDING FOR AN EFFECTIVE DATE.

WHEREAS, the City of Deltona, Florida has received an application to rezone approximately 102.7 acres from Residential Planned Unit Development (RPUD) to Residential Planned Unit Development (RPUD); and

WHEREAS, the City of Deltona, Florida and its Land Planning Agency have complied with the requirements of the Municipal Home Rule Powers Act, sections 166.011 et. seq., Florida Statutes, in considering the proposed RPUD rezoning; and

WHEREAS, after said public hearing, the City Commission of the City of Deltona, Florida, has determined that the RPUD zoning is consistent with the Comprehensive Plan of the City of Deltona, Florida.

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COMMISSION OF THE CITY OF DELTONA, FLORIDA, as follows:

Section 1. Located in the City of Deltona, Florida the following property is hereby rezoned to RPUD:

OR 6801, PAGE 3579

A portion of the North 1/2 of Section 12, Township 19 South, Range 31 East, Volusia County, Florida, more particularly described as follows:

Commence at the North 1/4 corner of said Section 12; thence South 00°12'06" East. 35.00 feet to the point of beginning; thence North 89°59'16" East, 1333.40 feet; thence South 00°24'06" East,

1285.85 feet; thence North 89°57'56" East, 1337.92 feet, to a point on the East line of the Northeast 1/4 of said Section 12; thence South 00°35'15" East, along said line 660.18 feet; thence South 89°57'15" West, 1340.18 feet; thence North 00°32'09" West, 480.44 feet; thence South 89°59'40" West, 178.79 feet; thence North 00°23'29" West, 180.04 feet; thence South 89°59'36" West, 1440.94 feet to a point on the East line of the West 31.0 acres of the Northeast 1/4 of the Northwest 1/4 of said Section 12; thence North 00°26'19" West, along said line, 1285.77 feet; thence South 89°59'21" East, 288.37 feet to the Point of Beginning Said lands lying in Volusia County, Florida.

Less

A portion of the North 1/2 of Section 12, Township 19 South, Range 31 East, Volusia County, Florida more particularly described as follows:

Commence at the Northeast corner of said Section 12; thence South 89°59'26" West, along the North line of the Northeast 1/4 of said Section 12, a distance of 2622.03 feet; thence South 00°12'15" East, 1004.55 feet; thence South 89°47'45" West, 104.82 feet to the Point of Beginning; thence South 00°12'15" East, a distance of 220.00 feet; thence South 89°47'45" West, 200.00 feet; thence North 00°12'15" West, 220.00 feet; thence North 89°47'45" East, 200.00 feet to the Point of Beginning. Said lands lying in Volusia County, Florida.

TOGETHER WITH:

A portion of Section 12, Township 19 South, Range 31 East, Volusia County, Florida, more particularly described as follows:

Commence at the Northwest corner of the Southwest one-quarter of the Northeast one-quarter of said Section 12; thence South 89°47'05" East along the North line of said Southwest one-quarter. 1257.97 FEET; thence South 00°03'57" East. 180.00 feet to the point of beginning; thence South 89°47'05" East, 80.00 feet, to a point on the West line of the Southeast one-quarter, of the Northeast one-quarter of said Section 12; thence South 00°03'57" East, along said line, 1532.84 feet to a point on the North right-of-way Line of Doyle Road; thence North 71°58'36" West, along said line, 84.16 feet; thence North 00°03'57" West, 1507.10 feet to the point of beginning.

AND TOGETHER WITH

OR 6969, PAGE 3583

THE WEST 31 ACRES OF THE NORTHEAST ONE-QUARTER OF THE NORTHWEST ONE-QUARTER OF SECTION 12, TOWNSHIP 19 SOUTH, RANGE 31 EAST, VOLUSIA COUNTY, FLORIDA MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCE AT THE NORTH ONE-QUARTER CORNER OF SAID SECTION 12;
THENCE NORTH 89°59'21" WEST, ALONG THE NORTH LINE OF THE NORTHWEST ONE-QUARTER OF SAID SECTION 12, A DISTANCE OF 288.63 FEET; THENCE SOUTH 00°26'19" WEST, 35.00 FEET TO THE POINT OF BEGINNING; THENCE SOUTH 00°26'19" EAST, A DISTANCE OF 1283.10 FEET; THENCE SOUTH 00°26'19" EAST, 2.68 FEET;

THENCE SOUTH 89°59'40" WEST, 1045.57 FEET; THENCE NORTH 00°26'19" WEST, 1321.08 FEET; THENCE SOUTH 89°59'21" EAST, 167.56 FEET; THENCE SOUTH 00°00'39" WEST, 35.00 FEET; THENCE SOUTH 89°59'21" EAST, 878.28 FEET TO THE POINT OF BEGINNING SAID LANDS LYING IN VOLUSIA COUNTY, FLORIDA.

Section 2. This Ordinance is adopted in conformity with and pursuant to the Comprehensive Plan of the City of Deltona, the Local Government Planning and Development Act, sections 163.161 et. seq., Florida Statutes, and the Municipal Home Rule Powers Act sections 166.011 et. seq., Florida Statutes.

Section 3. Conflicts. Any and all Ordinances or parts of Ordinances in conflict herewith are hereby repealed.

Section 4. Severability. If any provision of this Ordinance or the application thereof to any person or circumstance is held invalid, such invalidity shall not affect any other provisions or applications of this Ordinance which can be given effect without the invalid provision or application.

Section 5. Effective Date. This Ordinance shall take effect immediately upon its final adoption by the City Commission.

PASSED AND ADOPTED BY THE CITY COMMISSION OF THE CITY OF DELTONA, FLORIDA THIS _____ DAY OF _____, 2016.

First Reading: _____

Advertised: _____

Second Reading: _____

BY: _____

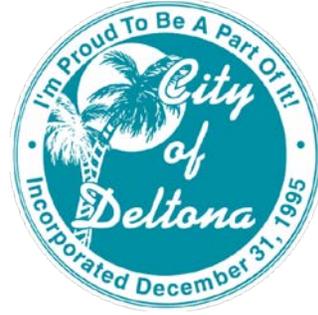
JOHN C. MASIARCZYK, SR., Mayor

ATTEST:

JOYCE RAFTERY, CMC, MMC City Clerk

Approved as to form and legality
for use and reliance of the City of
Deltona, Florida

GRETCHEN R. H. VOSE, ESQ, City Attorney



Staff Report

To: Planning and Zoning Board

From: Chris Bowley, AICP, Director Planning and Development Services

Date: May 26, 2016

Re: Ordinance No. 26-2016, Project No. RZ16-001, Deltona Free Standing Emergency Room, Business Planned Unit Development

A. Summary of Application:

Applicant: Matthew West, AICP. Littlejohn Engineering Assoc. Inc. representing Central Florida Regional Hospital

Request:

Tax Parcel No.: a portion of 8106-04-00-0542

Property Acreage: ± 3.24 Acres

Property Location: The property is located in the southwestern corner of the intersection of Howland Blvd. and Graves Av. situated within the Activity Center.

Legal Description: COMMENCE AT THE SOUTHEAST CORNER OF LOT 70, YOURLANDO FARMS AND GROVES PLAT (PLAT BOOK 10, PAGES 227 AND 228 OF THE OFFICIAL RECORDS OF VOLUSIA COUNTY, FLORIDA; THENCE ALONG THE EAST LINE OF PLATED LOT 70, NORTH 00°18'37" WEST A DISTANCE OF 10.10 FEET TO THE NORTHERLY RIGHT OF WAY LINE OF GRAVES AVENUE AND THE POINT OF BEGINNING; THENCE ALONG SAID NORTHERLY RIGHT OF WAY LINE, NORTH 89°23'10" WEST A DISTANCE OF 292.39 FEET; THENCE LEAVING SAID NORTHERLY RIGHT OF WAY LINE, NORTH 00°37'08" EAST A DISTANCE OF 381.01 FEET; THENCE NORTH 89°22'52" WEST A DISTANCE OF 50.00 FEET; THENCE NORTH 00°37'08" EAST A DISTANCE OF 150.00 FEET; THENCE NORTH 55°56'49" EAST A DISTANCE OF 114.36 FEET TO THE SOUTHWESTERLY RIGHT OF WAY LINE OF HOWLAND BLVD (S.R. 472); THENCE ALONG SAID SOUTHWESTERLY RIGHT

OF WAY LINE ALONG A CURVE TO THE LEFT AND BEING CONCAVE NORTH EAST HAVING A RADIUS OF 1748.64 FEET, A CENTRAL ANGLE OF 12°32'22" AND AN ARC LENGTH OF 382.70 FEET, THE CHORD OF SAID CURVE BEARS SOUTH 38°58'46" EAST A DISTANCE OF 381.93 FEET TO THE AFOREMENTIONED EASTERLY LINE OF LOT 70; THENCE SOUTH 00° 18'37" EAST A DISTANCE OF 301.79 FEET TO THE POINT OF BEGINNING..

B. Existing Zoning: Interchange Commercial Center Business Planned Unit Development (BPUD)

C. Background: The 3.24 acre property is a portion of a larger approximately 29 acre parent parcel. The 29 acre parent parcel was rezoned to Business Planned Unit Development (BPUD) in 2006 which was known as the Interchange Commercial Center. Even though the City adopted the rezoning Ordinance (Ordinance No. 22-2004), the BPUD Development Agreement was never executed. Recently, the City accepted an application to rezone 3.24 acres of the Interchange Commercial Center BPUD. Essentially, the present BPUD request will replace the Interchange Commercial Center BPUD for the 3.24 acres.

The use proposed is referred to as a freestanding emergency room. A freestanding emergency room is a relatively new medical land use and is a stand-alone facility where emergency medical care is provided. Other services include laboratories and imaging. The purpose of a freestanding emergency room is to provide fast and convenient emergency medical care. However, while freestanding emergency rooms may feature limited care beds, these facilities are not considered hospitals.

D. Support Information Public Facilities:

- a. Potable Water: to be supplied by Volusia County Utilities
- b. Sanitary Sewer: to be supplied by Volusia County Utilities
- c. Fire Protection: City of Deltona – Station 65
- d. Law Enforcement: Volusia County Sheriff's Office (VCSO)
- e. Electricity: Duke Energy

E. Matters for Consideration:

Section 110-1101, Code of City Ordinances, states that the City shall consider the following matters when reviewing applications for amendments to the Official Zoning Map:

1. Whether it is consistent with all adopted elements of the Comprehensive Plan.

The property is located within a local planning area of the City referred to as the Activity Center. The Activity Center is strategically located at the SR 472/I-4 interchange and has

the potential to provide needed economic development for the City of Deltona and southwest Volusia County. To realize the economic potential of the area, the Activity Center is associated with special planning and land use provisions.

OBJECTIVE FLU2-1

Achieve an integrated and well-planned mixture of urban land uses within The Deltona Activity Center that encourages the creation of an employment center.

The above objective illustrates the Activity Center is anticipated to be an employment center for the City. Medical uses typically are associated with higher wage skill sets oftentimes with benefits. Therefore, the rezoning would be consistent with this provision.

Policy FLU2-1.7

Future development within the Activity Center shall require rezoning to a Planned Unit Development (PUD) or amendment to an existing PUD. Provided, however, that any development of an existing parcel that is 0.5 acres or less in size and which is permissible by the existing zoning classification assigned the parcel shall not require rezoning to PUD if the existing zoning classification is consistent with the Activity Center Plan future land use designation.

The Business Planned Unit Development rezoning process for the property is required by this policy

Policy FLU2-1.13

Individual developments within the Activity Center shall be designed consistent with the City of Deltona's Urban Design Pattern Book and Urban Design Master Plan as it may be updated from time to time to provide visual compatibility and functional continuity with other adjacent developments within the Activity Center.

The project will be subject to the aesthetic requirements of this policy: Promote well designed and high quality development forms within the Activity Center.

The proposed development is also required to be consistent with the remaining Comprehensive Plan.

Policy FLU1-1.11

The following public facilities and services shall be available for new development in all areas: roadways, solid waste collection, stormwater management, fire and police protection, emergency medical services, potable water, sanitary sewer service, and public schools as defined in the Public School Facilities Element.

The project will be served by adequate infrastructure including transportation. To address transportation improvements programed under a previously approved BPUD (Deltona Village), the project will be subject to traffic mitigation requirements to improve Graves Av.

Policy FLU1-7.5

Subdivisions shall be designed so that all individual lots have access to the internal street system network with peripheral lots buffered from major roads and incompatible land uses.

The BPUD rezoning does involve a subdivision and includes the beginning of an internal road system.

Policy T1-1.4

The City of Deltona shall maintain land use regulations to facilitate safe and efficient transportation, which operates within the accepted levels of service as adopted by the City.

Access off of Graves Av. will be limited to right in and right out movements. In addition, there is a requirement for the applicant to construct a raised median between the Graves Av. travel lanes. The intent of the raised median is to discourage left turns. The suggested improvements will promote safe ingress and egress to the facility and ensure appropriate traffic flow.

2. Its impact upon the environment or natural resources.

The property is associated with well drained, sandy soils. There are no wetlands on the property and none of the area proposed to be rezoned is located within the 100 year flood plain.

The property has been cleared in the past. The property can be described as a ruderal landscape comprised of scrub palmetto, oak saplings, dogfennel, broom grass, and various early successional weeds. While the property may have been cleared, the site may be suitable habitat for gopher tortoises and scrub jays. It is not unusual for mechanically manipulated areas to support populations of scrub jays and/or gopher tortoises. To determine if the property supports scrub jays or gopher tortoises, the applicant will be required to survey the property. If gopher tortoises are noted, then the tortoises will need to be relocated or preserved on site as per State permitting requirements. Scrub jays are a listed species protected under the Endangered Species Act. Scrub jays and related habitat are regulated by the U.S. Fish and Wildlife Service and any impact to scrub jays or habitat will be subject to a Habitat Conservation Plan.

All listed species surveying and applicable permitting will be addressed during the site plan review process.

3. Its impact upon the economy of any affected area.

The proposed facility is a medical use and medical jobs typically feature relatively high wages, stable employment and oftentimes benefit packages. The facility will be staffed by approximately 30 personnel and will be open 24 hours a day. There is anticipation staff will operate in shifts. The project will bring in employment which has the potential to increase demand for retail service uses including restaurants.

4. Notwithstanding the provisions of Article XIV of the Land Development Code, Ordinance No. 92-25 [Chapter 86, Code of Ordinances] as it may be amended from time to time, its impact upon necessary governmental services, such as schools, sewage disposal, potable water, drainage, fire and police protection, solid waste or transportation systems.

- a. **Schools:** N/A. The project is not residential and would have no impact on school workstation space.
- b. **Sewage Disposal:** Volusia County utilities will serve the project with central sewer. Currently, the County has over 350,000 gallons per day of wastewater treatment capacity available. Therefore there is ample capacity to serve the project.
- c. **Potable Water:** Volusia County utilities will serve the project with potable water. The County is currently providing a high potable water level of service within the Deltona North service area. The County does have the capacity to serve the property with potable water.
- d. **Drainage:** Stormwater will be managed in accordance with the City Land Development Code and the St. Johns River Water Management District.
- e. **Transportation Systems:** A freestanding emergency room is a new medical land use. The use is intended to provide quick and convenient emergency medical service. Emergency room service has typically been ancillary to hospitals where a full range of medical services are provided within a large central facility. Traffic generation rates associated with hospitals have been well studied by the Institute of Traffic Engineers and others. The trip generation characteristics of hospitals tend to be somewhat unique based on shift changes, visitation hours, patient

admission/discharges, etc. These characteristics are driven, in part, because a hospital tends to be open seven days a week and 24 hours a day.

Conversely, traffic generation rates for a freestanding emergency room have not been well documented. Since the freestanding emergency room has some commonalities with a hospital – 24/7 operation, medical services, etc., the hospital traffic generation rates were applied to determine traffic impacts associated with the proposed freestanding emergency room use.

Utilizing the above referenced methodology revealed the almost 11,000 square foot freestanding emergency room use would generate 143 trips with a pm peak of 10 movements to and from the facility. The trip generation rate of the use is well below the typical 1,000 trip threshold to perform a traffic impact analysis (TIA). However, the project is located in a congested area of the City. While none of the immediate roadway segments are failing, Graves Av., SR 472, and Howland Blvd. near the project are close to capacity. Furthermore, the local roadway geometry, signalization and traffic patterns create a condition where safety is of paramount importance. Notwithstanding the local roadway network has the capacity to support the 143 trips, the project needs to be carefully managed with regard to intensity, access and off-site transportation mitigation.

A road providing access to Graves Av. from the facility is planned. The access movement for the road will be limited to right in and right out. The intent of limiting the movement is because of the local curvature of Graves Av. which limits the view of on coming traffic. In addition, to ensure the right in and right out access is respected by drivers, the applicant will be required to install a raised concrete median down the center of Graves Av. near the project. The dimensions of the median will be determined by the County through the County Use Permit process (Graves Av. is a County Road).

In 2010, the City approved the Deltona Village Business Planned Unit Development. The Deltona Village BPUD mostly addressed 130 acres located south of the freestanding emergency room property. The Deltona Village BPUD transportation mitigation section addressed not only the 130 acres located south of Graves Av. but the 29 acre site of which the 3.24 acre property is a part. According to the Deltona Village BPUD, the 3.24 acre area is situated in “County Phase 1”. The subject phase is programed to provide mitigation. The mitigation associated with the Deltona Village BPUD mostly includes improvements to Graves Av. including a new Graves Av. west bound travel lane. The scope and size of the project will not trip the threshold for immediate improvements included within the Deltona Village BPUD. However, the owner and/or successor(s) in title is/are responsible, as identified in the Deltona Village Development Agreement, to finance and implement transportation improvements. The above mentioned access road featuring the right in and right out movement is one of three access points originally proposed by the applicant. The other two access points involve extending roads across land not proposed to be bought by

the applicant. One access point would intersect at a right angle with Normandy Blvd. at the Graves Av./Normandy Blvd. intersection. The other proposed access point was located further west towards the Graves Av. I-4 overpass. Both of these access points have merit and may be more functional than the right in right out access point planned to serve the property. However, the other access points involve unknown dynamics including ownership characteristics and other variables not suited to being adequately addressed under the subject BPUD document. Therefore, if access does change or is modified, the revised access network will need to be addressed through a PUD amendment or perhaps a new PUD involving abutting or nearby land.

5. Any changes in circumstances or conditions affecting the area.

None.

6. Any mistakes in the original classification.

No known mistakes.

7. Its effect upon the public health, welfare, safety, or morals.

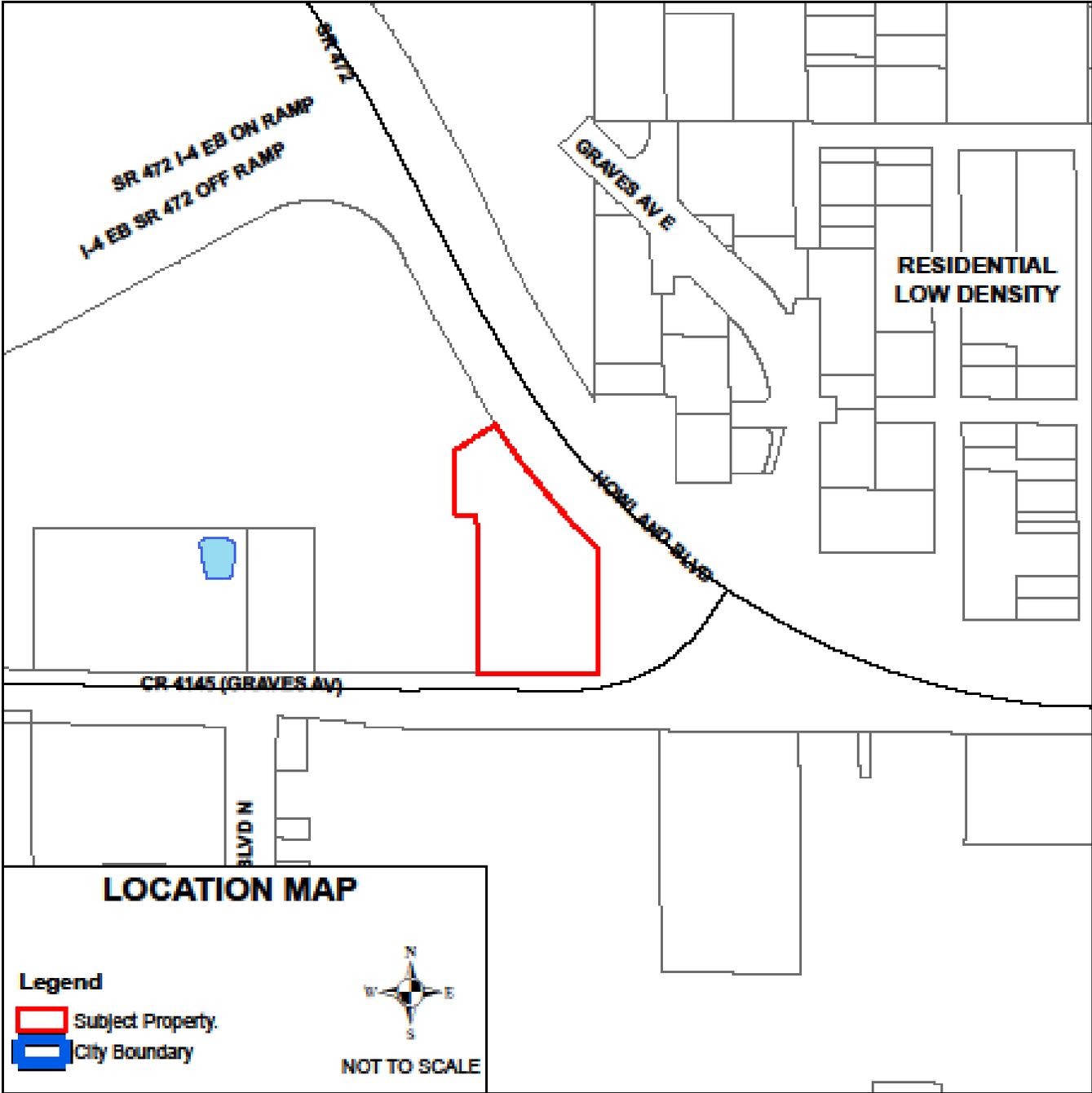
All Planned Unit Development zoning actions are associated with a written Development Agreement (DA). The DA establishes certain land uses, intensities and other development parameters intended to implement the mission of government – health, welfare, safety and morals. The DA is attached. The language of the DA suggested by City Staff is presented in an underline and strike through format.

Highlights of the recommended changes to the DA include the following:

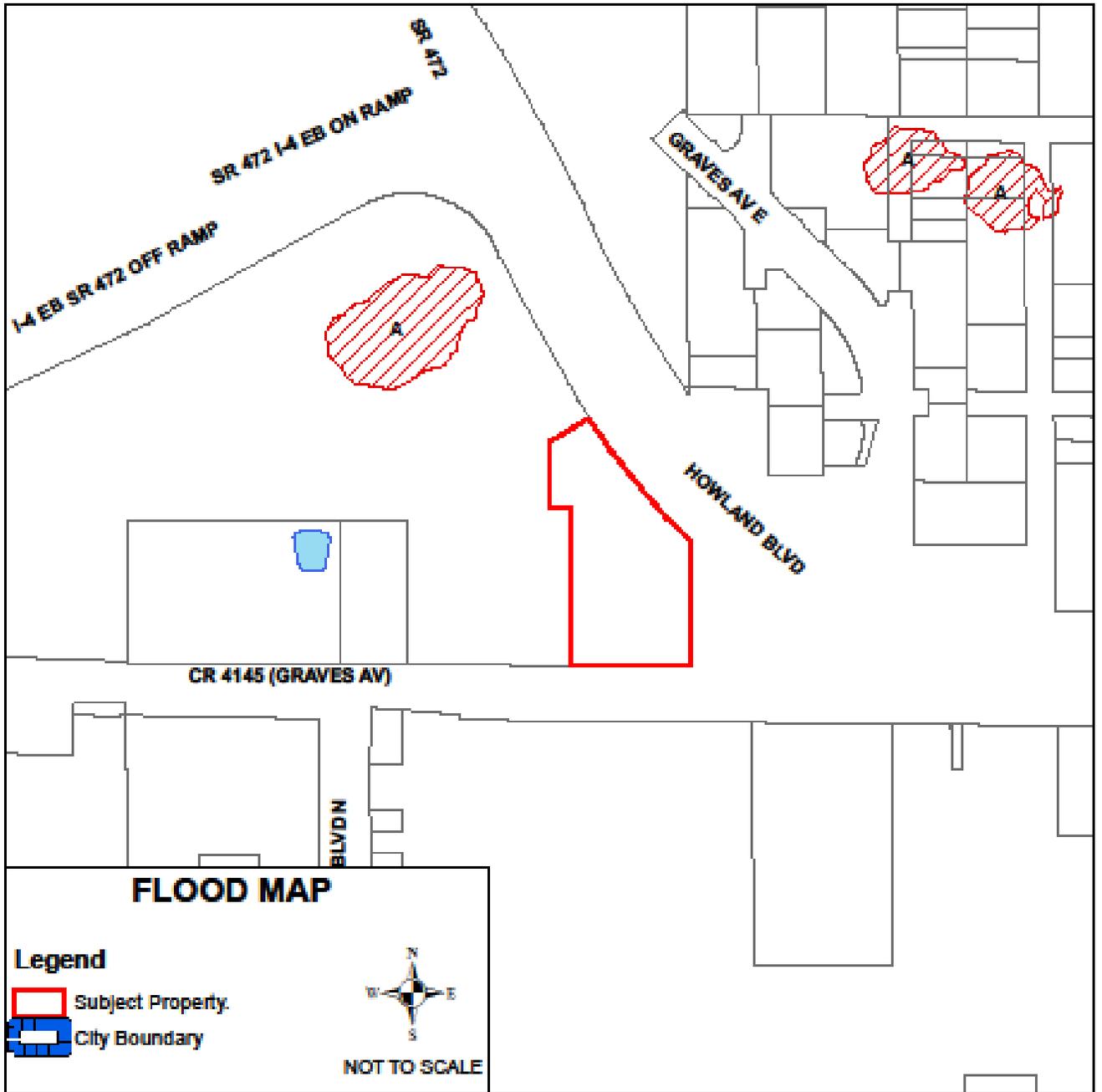
- a. Limitation on land use to a freestanding emergency room;
- b. Limitation on the intensity to reflect applicant proposal;
- c. Addition of aesthetic standards;
- d. Enumeration of various transportation conditions including access management;
- e. To recognize the 3.24 acre area as a legal lot, the underlying plat needs to be vacated and the 3.24 acre area platted; and
- f. The addition of language regarding scrub jay and gopher tortoise protection.

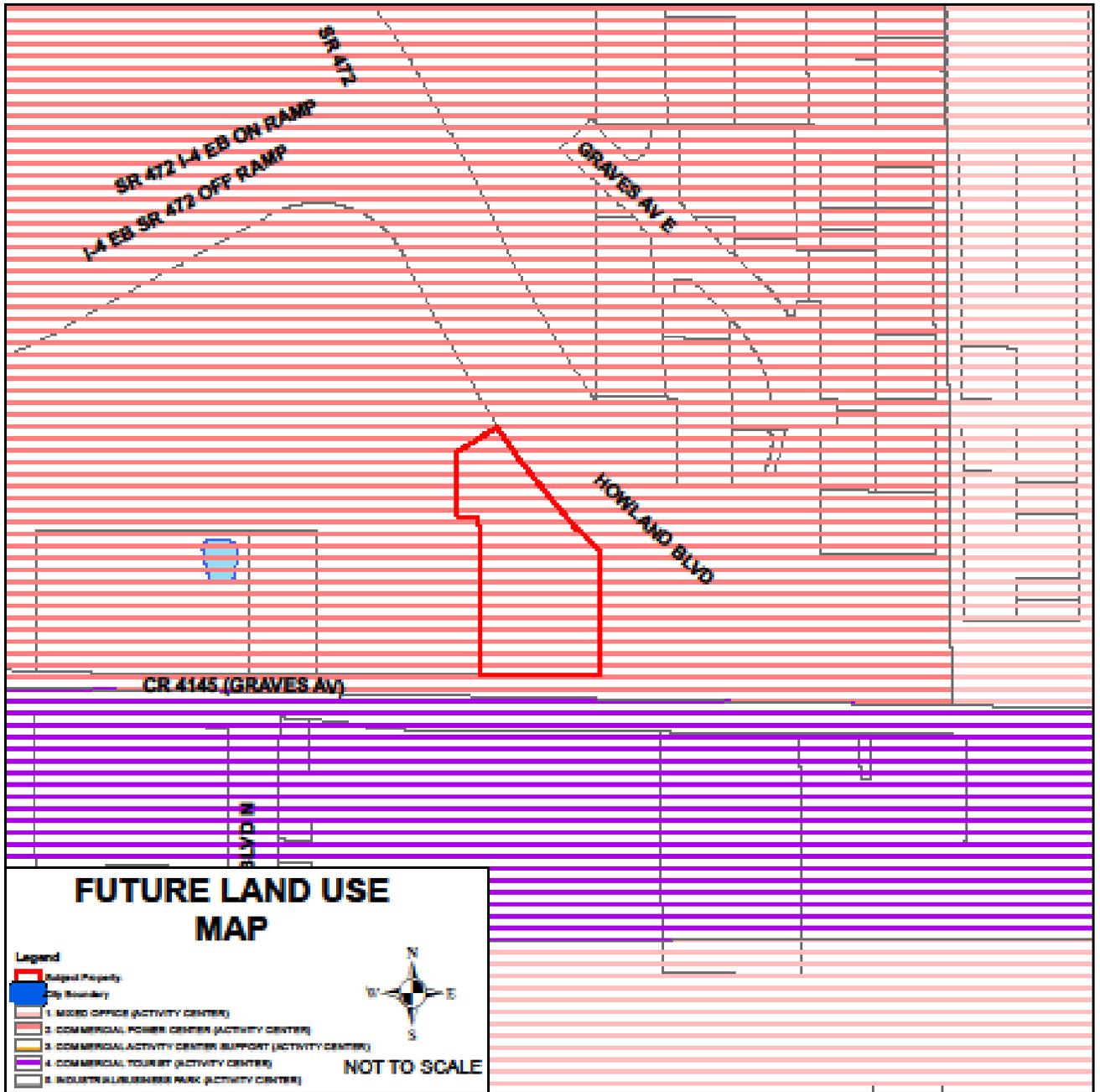
CONCLUSION/STAFF RECOMMENDATION:

The proposed freestanding emergency room use is consistent with the City vision for the Activity Center. The freestanding emergency room will create jobs with good wages and benefits. In addition, the rezoning seems to further a medical oriented land use trend for the Activity Center that started with the Halifax Hospital development located in the vicinity of the proposed freestanding emergency room. There is anticipation the initial medical uses in the Activity Center will promote ancillary medical development including doctor offices, etc. In addition, increased employment in the area will spur the development of services like restaurants. Finally, the proposed rezoning is consistent with the Comprehensive Plan and can be served by existing public services. Therefore, staff recommends the adoption of Ordinance 26-2016, rezoning the 3.24 acres to Business Planned Unit Development.









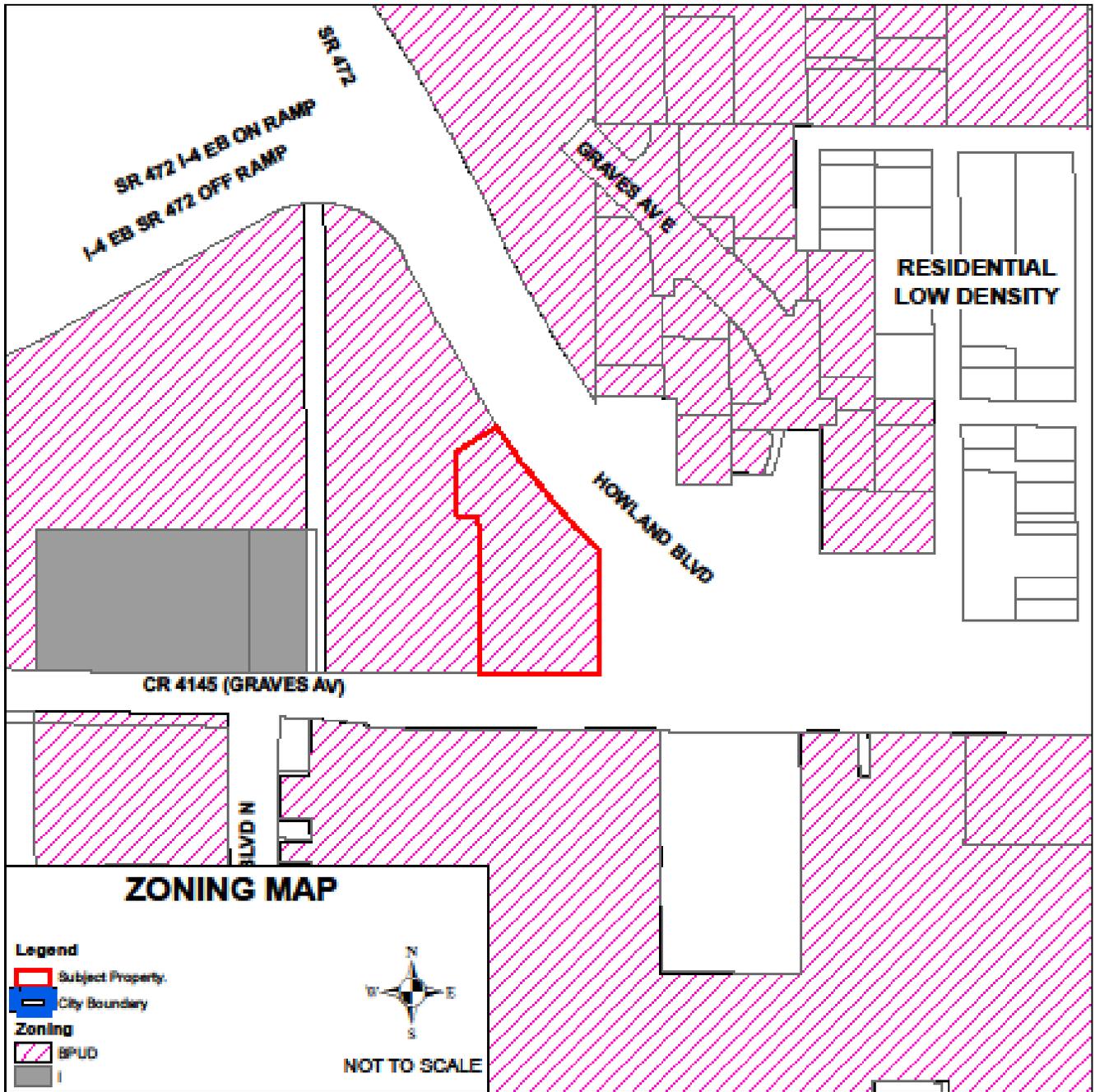
SR 472
 SR 472 I-4 EB ON RAMP
 I-4 EB SR 472 OFF RAMP

GRAVES AVE

HOWLAND BLVD

CR 4145 (GRAVES AV)

BLVD W



**THIS INSTRUMENT PREPARED BY
AND AFTER RECORDING RETURN TO:**

Gretchen R. H. Vose, Esq.
City Attorney
City of Deltona
2345 Providence Boulevard
Deltona, Florida 32725

<p>For Recording Purposes Only</p> <p>Signature of Notary</p>
--

Exhibit "A" to Ordinance No. 26-2016

DEVELOPMENT AGREEMENT

for the project known as the Deltona Freestanding Emergency Room (FSER) Business Planned Unit Development (BPUD) located at the ~~northwest~~ southwest corner of Graves Avenue and Howland Boulevard (hereinafter referred to as the "Subject Property").

THIS DEVELOPMENT AGREEMENT (hereinafter referred to as the "Agreement") is entered into and made as of the ___ day of _____, 2016, by and between the CITY OF DELTONA, a Florida municipal corporation, with a mailing address of 2345 Providence Boulevard, Deltona, Florida 32725, (hereinafter referred to as the "City"), and William F. DeMarsh, (hereinafter referred to as the "Owner or Owner/Developer"), and Central Florida Hospital, (hereinafter referred to as the "Developer", ~~if the Developer is a separate entity from the "Owner/Developer" and the Developer has an executed Notarized Owner Authorization from the Owner/Developer~~).

WITNESSETH

WHEREAS, the Owner warrants that it holds legal title to the lands located in Volusia County, Florida, and within the corporate limits of the City of Deltona, said lands being more particularly described in Exhibit "B", Legal Description for the Subject Property, attached hereto and by this reference made a part hereof; and that the holders of any and all liens and encumbrances affecting such property will subordinate their interests to this Agreement; and

WHEREAS, the Owner/~~Developer~~ has clear title of the Subject Property or the Developer is currently under contract to purchase the Subject Property and intends to develop such property as a planned commercial development; and

WHEREAS, the Owner/~~Developer~~—or Developer desires to facilitate the orderly development of the Subject Property in compliance with the policies, laws and regulations of the City and of other governmental authorities, and the Owner/~~Developer~~ or Developer desires to ensure that its development is compatible with other properties in the area and planned traffic patterns; and

WHEREAS, the development permitted or proposed under this Development Agreement is consistent with the City’s Comprehensive Plan, concurrency management system, and all land development regulations and this Agreement does not replace, supersede, or grant variances to those regulations; and

WHEREAS, it is the purpose of this Agreement to clearly set forth the understanding and agreement of the parties concerning the matters contained herein; and

WHEREAS, the Owner/~~Developer~~ and/or Developer have sought the City's approval to develop the Subject Property, and the City approved Ordinance No. 26-2106, through rezoning the Subject Property to a ~~form of~~ Business Planned Unit Development (BPUD), as defined under the City’s Land Development Code on _____. The BPUD shall consist of this Agreement as the Written Agreement of the BPUD and an Exhibit “C”, Master Development Plan (MDP), attached hereto and by this reference made a part hereof as the Preliminary Plan, subject to the covenants, restrictions, and easements offered by the Owner/~~Developer~~ or Developer and contained herein, (hereinafter the “Master Development Plan”). Where more detailed criteria for City required submittals exceed the criteria required for a Master Development Plan, the more detailed criteria applies.

NOW THEREFORE, in consideration of the mutual covenants and agreements contained herein, and other good and valuable consideration, the receipt and sufficiency of which is hereby acknowledged, the parties hereto agree as follows:

1. **Recitals and Definitions.** The recitals herein contained are true and correct and are incorporated herein by reference. All capitalized terms not otherwise defined herein shall be as defined or described in the City’s Land Development Code as it may be amended from time to time, unless otherwise indicated.

2. **Ownership.** The legal and equitable owners of the Subject Property is are: William F. DeMarsh.

3. **Title Opinion/Certification.** The Developer will provide to the City, in advance of the City's execution and recordation of this Agreement, a title opinion from a licensed attorney in the state of Florida, or a certification by an abstractor or title company authorized to do business in the state of Florida, verifying marketable title to the Subject Property to be in the name of the Owner/~~Developer~~ and any and all liens, mortgages, and other encumbrances that are either satisfied or not satisfied or released of record.

4. **Subordination/Joinder.** Unless otherwise agreed to by the City and if applicable, all liens, mortgages, and other encumbrances ~~that is~~ not satisfied or released of record, must be subordinated to the terms of this Agreement or the Lienholder join in this Agreement. It shall be the responsibility of the Owner/~~Developer~~ and/or Developer to promptly obtain the said subordination or joinder, in form and substance that is acceptable to the City Attorney, prior to the execution and recordation of this Agreement.

5. **Duration.** The duration of this Agreement is binding and runs with the land in perpetuity, unless amended.

6. **Development of the Subject Property.** Development of the Subject Property shall be subject to performance standards listed in this Agreement. Where a land use listed below differs from a defined use in the City of Deltona's Code of Ordinances, the use listed in this Agreement shall prevail.

A. Comprehensive Plan Policies specific to this Subject Property: None
~~None.~~

B. Permitted principal uses allowable on the Subject Property shall be: ~~Permitted and Conditional Uses for C-2, General Commercial (Sec 110-316, City of Deltona Land Development Code) except as noted under Section C. Freestanding Emergency Room. A free standing emergency room is a standalone medical facility featuring emergency care services, imaging, laboratory activities, and related administrative uses. Freestanding Emergency Room~~

~~B. Prohibited principal uses, if any: Any use besides a freestanding emergency room as described above.~~

~~Land uses not specifically listed as permitted are prohibited. In addition this ordinance shall not be interpreted to permit the following land uses as either temporary or permanent principal or accessory uses~~

~~1. Sale of sparklers or fireworks~~

~~2. Carnivals fairs and circuses~~

~~3. Automobile used car sales except as an accessory use to permitted sales of new~~

- ~~cars and arts and crafts shows except as permitted by Section 110-814.11 Code of Ordinances of the City of Deltona Florida as it may be amended from time to time~~
- ~~4. Outdoor storage except at the rear of the main building in areas designed for that purpose and screened from view from on or off the site~~
- ~~5. Fireworks displays and lightshows~~
- ~~6. Outdoor music concerts or musical promotions~~
- ~~7. Driving or Riding Schools or instructional classes~~
- ~~8. Display of boats motors, appliances, arts and crafts, vehicles, pets, livestock and similar items for sale or hire except as permitted by Section 110-814.11 of the Code of Ordinances of the City of Deltona and the Owner Developer~~
- ~~9. Communication towers and antennas~~
- ~~10. Adult entertainment establishments~~
- ~~11. Boarding houses~~
- ~~12. Tattoo Parlors~~

- C. Proposed ~~minimum density (in number of dwelling units per acre) or minimum intensity (measured in floor area ratio): 0.1 minimum no minimum FAR 0.077 FAR~~
- D. Proposed maximum density ~~(in number of dwelling units per acre) or maximum intensity (measured in floor area ratio), if any: 0.55 0.08 maximum FAR. The facility shall be limited to a maximum of 12 private care patient beds.~~
- E. Impervious surface ratio is not to exceed 70% of the gross square footage for the Subject Property.
- F. Maximum lot coverage ~~(in %) (dry retention systems can be used towards open space): 75% 8%~~
- G. Minimum landscaping and buffer yard requirements shall be consistent with Sec. 110-808 as it may be amended from time to time. are per the City's Land Development Code. ~~Stormwater management facilities shall not be placed within buffer yards.~~
- H. Minimum lot size area ~~(in acreage or square footage): 1.0 3.24 acres~~
- I. Minimum lot width ~~(in feet): 100 feet~~
- J. Minimum yard setbacks:
 - 1. Front yard: 35 Feet
 - 2. Side yard: 5 Feet
 - 3. Street side yard: ~~25~~ 35 Feet
 - 4. Rear yard: 15 Feet

- K.** Maximum building height (~~in feet~~): 75 35 Feet
- L.** Minimum parking standards are per Sections. 110-828 and 110-829 of the City's Land Development Code as may be amended from time to time.
- M.** Minimum lighting standards per the City's Land Development Code shall be included on a separate Illumination Plan to be provided at the time of site plan submittal.
- N.** Signs: Signs shall comply with Chapter 102 of the City Land Development Code as it may be amended from time to time.
- O.** Architectural controls and development on the Subject Property shall follow a common architectural theme and reflect a general appearance consistent with the City of Deltona Urban Design Pattern Book. The intent is to establish a high level of aesthetic within the Activity Center. For purposes herein, general appearance includes, but is not limited to, exterior wall finishes, construction materials, roof style, architectural details and ornamentation. The following requirements shall apply to development on the subject property:
1. The structure shall convey a sense of quality and permanence;
 2. Exterior walls facing public rights-of-way shall be constructed of finish materials such as stucco, natural brick or stone, finished concrete, or wood;
 3. All accessory structures, and signage shall be consistent with the overall theme of the principal use;
 4. Canopies shall be compatible with the architectural design of the principal structure and be designed consistent with other Fire Code and public safety oriented requirements;
 5. All service areas and mechanical equipment visible from public rights-of-way (ground or roof) including, but not limited to, air conditioning condensers, heating units, electric meters, satellite dishes, irrigation pumps, and other mechanicals/utilities shall be screened using architectural features consistent with the structure or landscaping of sufficient density and maturity at planting to provide opaque screening. ~~as listed in this Agreement by harmoniously coordinating the general appearance of all buildings and accessory structures. All controls and variations shall be defined by a Homeowners Association or Property Owners Association, as defined within this Agreement.~~
- P.** Utility provision and dedication: ~~The Owner/Developer or Developer shall connect to the City of Deltona's central utility systems, when available, or to Volusia County's central utility systems, where applicable,~~ at their sole cost and expense. Utility fees shall be paid to ~~Deltona Water or Volusia County, respectively,~~ before any building

permit is issued. Central utility systems are to be designed, permitted, and constructed to the respective service provider specifications and dedicated to the respective service provider upon final inspection, clearance, and acceptance by the service provider.

Q. ~~Stormwater and environmental: Per parcel stormwater systems or master~~ The stormwater system shall be owned and maintained by an established Homeowners Association or Property Owners Association the owner in private ownership and shall not be dedicated to or become the responsibility of the City of Deltona. All environmental permitting, mitigation, and/or soil and erosion control for the property shall conform to all federal, state, and local permits/requirements; shall be the sole responsibility of the owner and/or developer; Homeowners Association or Property Owners Association, and shall be maintained in good condition/standing with the applicable permitting authorities. Best Management Practices and conformance to National Pollutant Discharge Elimination System (NPDES) criteria are required.

~~**R.** Transportation, site access, and traffic devices: The Owner/Developer or Developer is responsible for all transportation improvements within the Subject Property and any off-site transportation requirements, as a result of the proposed development, for site function, that maintains or improves the level of service for area roadways, and ensures the public health, safety, and welfare for the community. All permits shall be obtained from appropriate permitting agencies prior to development and the City shall determine the appropriate level of service per the City Comprehensive Plan and current traffic counts. The Subject Property was included within the Traffic Impact Study (TIS) submitted to Volusia County (December 2009), which covered 804,000 square feet out of the 900,000 square feet in the Deltona Village BPUD and the Interstate Commerce Center (which was never recorded as a BPUD). A separate TIS was submitted to the City of Deltona (dated December 2009) which covered 96,000 square feet along the southern boundary of the Deltona Village BPUD. The subject property is geographically referenced in the TIS and therefore, it is the interpretation of the City of Deltona that the transportation provisions in the Development Agreement for the Deltona Village BPUD (Book 6482, Page 4234) apply to the Subject Property. The Subject Property is considered a sub-phase of the "County TIA Phase I" and will be responsible for proportionate share payments as outlined in sections 10 and 11 of the Development Agreement for the Deltona Village BPUD (Book 6482, Pages 4240-4245). The mitigation required includes payment towards making improvements to Graves Av. as outlined in the Deltona Village BPUD. Before a Certificate of Occupancy is issued for the project, the required mitigation must be accounted to the satisfaction of the County and City.~~

S.R.

Based on a determination that a freestanding emergency room facility traffic generation rates are analogous to a hospital, the free standing emergency room use planned will generate 143 daily trips and no more than 10 peak hour trips. Therefore, a traffic impact analysis (TIA) consistent with the River to Sea Transportation Planning Organization (TPO) was not required for the project. Any change of use, increase in size or any other increase of intensity shall require a major amendment to this BPUD document and a TIA will be required to be prepared consistent with TPO guidelines.

Access to the Deltona FSER BPUD shall be provided as follows: in accordance with the following provisions as illustrated on Exhibit "D" of this Development Agreement:

~~**Access 1:** The Deltona FSER BPUD site shall utilize Access 1 as its main point of access to be granted direct access Graves Avenue. Access 1~~ The access shall be limited to right-in/right-out turning movements. This access point and its northerly extension as shown on the Master Development Plan shall be constructed in conjunction with the development on the site and shall be completed prior to issuance of any Certificate of Occupancy for the site. Access 1 The access and the northern extension of the Access 1 driveway access shall be a dedicated road and designed to facilitate access to other property in the area. as a cross-access driveway between the owners of the FSER BPUD and the owners of the remaining parent parcel. Drainage for Access 1 the access will be incorporated into the stormwater design for the Deltona Freestanding Emergency Room BPUD site. The access connection with Graves Av. shall be constructed with a raised, tapered concrete median ('pork chop') to maintain appropriate traffic flow. In addition, the owner or developer will be required to construct a raised concrete median of sufficient height, width and length within the median of Graves Av. to effectively discourage left turning movements. The design of the raised median shall be determined through the County Use Permit process but shall not include the use of quick curb or similar type of apparatus. All traffic management improvements associated with this access shall be completed before a Certificate of Occupancy is issued for the property.

~~**Access 2:** The Deltona FSER BPUD site may utilize Access 2 as an alternate, additional access point. The extents of Access 2 shall be included within a dedicated access easement road and will be constructed at the expense of the developer(s) of the Deltona FSER BPUD site and the remaining parent parcel. Drainage for Access 2 will need to be accommodated in master stormwater pond(s) on the parent parcel within a drainage easement. Utilities to serve the parent parcel and the Deltona FSER BPUD site may be constructed along Access 2 within a utility easement which should be addressed in a cost sharing agreement between the parent parcel owners and the Deltona FSER BPUD site owners. If Access 2 is going to be designed as a full~~

~~intersection movement, a signal warrant study will be required to determine when the intersection will need to be signalized. An element of the signal warrant study will be a financing plan needed to facilitate installation of the signal. However, the signal will not be responsible for any contributions towards construction of the signal or improvements appurtenant to. In addition, the signal warrant study will be required if and when Access 2 is proposed for construction. The signal warrant study will be the responsibility of the owner or developer or other future interests in the property to produce. The owner, developer or other private interest in the property responsible for the signal warrant study shall also be responsible for City consultant fees associated with the review of the signal warrant study.~~

~~**Access 3:** The Deltona FSER BPUD site may utilize Access 3 as an alternative to Access 2. The extents of Access 3 shall be included within a dedicated access easement road and will be constructed at the expense of the developer(s) of the Deltona FSER BPUD site and the owner(s) of the remaining parent parcel. Drainage for Access 3 shall be accommodated in master stormwater pond(s) on the parent parcel within a drainage easement. Utilities to serve the parent parcel and the Deltona FSER BPUD site may be constructed along Access 2 within a utility easement which should be addressed in a cost sharing agreement between the parent parcel owners and the Deltona FSER BPUD site owners.~~

~~Access 3 is predicated on acquiring land from property owner(s) other than the parent parcel (the concrete plant) to accommodate a true, right angle alignment with Normandy Boulevard and Graves Avenue. This will also require signal modifications which shall be provided for at the expense of the developers of the Deltona FSER BPUD site and/or the parent parcel owner(s).~~

~~There shall be not access permitted to Howland Boulevard/SR 472. Other access points may be contemplated in the future. Any other access points serving the development will need to be addressed through a major amendment to this BPUD unless the new access points are associated with another BPUD rezoning request.~~

~~The access roadway and other related improvements shall be constructed to City and, if applicable, County specifications and standards.~~

~~As depicted on the MDP, there is an extensive internal driveway circulation planned for the project. The internal circulation system, including driveway entrances on to the access road may need to be relocated and/or redesigned to afford appropriate spacing and promote safe and functional traffic patterns. Therefore, while the MDP depicts certain driveway cuts and internal circulation, those traffic circulation features may need to be redesigned to comply with City Land Development Code requirements.~~

~~F.S.~~ There shall be no limit on the hours of operation of a Free Standing Emergency Room use.

7. **Public Facilities/Land Dedication.** Facilities or tracts that either are or shall become public facilities/tracts that will serve the development and/or are on the Subject Property are, as follows: Roadways and utility infrastructure as described elsewhere in this Development Agreement.

8. **Development Permits/Fees.** The Owner/Developer or Developer is responsible for obtaining, permitting, and the payment of all fees for facilities and services ~~to ensure~~ for the Subject Property. Any site permits shall be kept current with the respective permitting agency and shall ensure the protection of the public health, safety, and welfare of the community and the development. All impact fees are applicable and no impact fee credits shall be awarded through this Agreement.; ~~unless a cessation exists through a City moratorium that is Citywide. Proportionate fair share site improvements shall not be used in lieu of impact fees.~~

9. **Obligations.** Should the Owner/~~Developer~~ or Developer fail to undertake and complete its obligations as described in this Agreement to the City's specifications, then the City shall give the Owner/~~Developer~~ or Developer thirty (30) days written notice to commence and ninety (90) days to complete said required obligation. If the Owner/~~Developer~~ or Developer fails to complete the obligations within the ninety (90) day period, then the City, without further notice to the Owner/~~Developer~~ or Developer, or its successors in interest, may, without prejudice to any other rights or remedies it may have, place liens and take enforcement action on the Subject Property. A lien of such assessments shall be superior to all others, and all existing lienholders and mortgagees, by their execution of the subordination or joinder documents, agree to subordinate their liens or mortgages to the City's said liens or assessments. Notice to the Owner/~~Developer~~ or Developer and its successors in interest shall be deemed to have been given upon the mailing of notice to the address or addresses set forth in Paragraph (23) hereof.

10. **Site Plan/Plat Approval.** Exhibit "C", the Master Development Plan, is the Preliminary Plan of the PUD and this Agreement. The Master Development Plan shall not replace, supersede, or absolve the Owner/~~Developer~~ or Developer from approvals for any site plan, preliminary plat, and/or final plat and their respective regulations. Where more detailed criteria for City required submittals exceed the criteria required for a Master Development Plan, the more detailed criteria applies. As per the City Land Development Code, the proposed lot will need to be platted and any development on the property will be required to be processed through the Final Site Plan review process. No site plan application will be approved unless and until the lot has received plat approval by the City Commission. At the request of the Owner or Developer, the City will process a Final Plat application at the same time as a Final Site Plan application. Before a Final Plat and/or Final Site Plan application(s) is/are submitted to the City, the Owner or Developer shall, at minimum, vacate lots 64 and 70 Yourlando Farms MB 10, PGS

227-228 consistent with Chapter 106 of the City Land Development Code.

11. **Indemnification.** The Owner/~~Developer~~ or Developer shall indemnify and hold the City harmless from any and against all claims, demands, disputes, damages, costs, expenses, (to include attorneys' fees whether or not litigation is necessary and if necessary, both at trial and on appeal), incurred by the City as a result, directly or indirectly, of the use or development of the Subject Property, except those claims or liabilities caused by or arising from the negligence or intentional acts of the City, or its employees or agents. It is specifically understood that the City is not guaranteeing the appropriateness, efficiency, quality or legality of the use or development of the Subject Property, including but not limited to, drainage or water/sewer plans, fire safety, or quality of construction, whether or not inspected, approved, or permitted by the City.

12. **Compliance.** The Owner/~~Developer~~ or Developer agrees that it, and their successors and assigns, will abide by the provisions of this Agreement, the City's Comprehensive Plan and the City's Code of Ordinances, including but not limited to, the site plan regulations of the City as amended from time to time, which are incorporated herein by reference and such subsequent amendments hereto as may be applicable. Further, all required improvements, including landscaping, shall be continuously maintained by the Owner/~~Developer~~ or Developer, or their successors and assigns, in accordance with the City's Code of Ordinances. The City may, without prejudice to any other legal or equitable right or remedy it may have, withhold permits, Certificates of Occupancy or plan/plat approvals to the Subject Property, should the Owner/~~Developer~~ or Developer fail to comply with the terms of this Agreement. In the event of a conflict between this Development Agreement and the City's Land Development Code, the more restrictive regulations shall govern the development of the Subject Property.

13. **Obligations for Improvements.** Any surface improvement as described and required hereunder included, but not limited to such as signalization, walls, stormwater management facilities, medians, and utilities, or any other surface improvement shall be performed, prior to the issuance of the first Certificate of Occupancy on that portion of the Subject Property that the surface improvement(s) relates or is otherwise scheduled in this Agreement. Should the Owner/~~Developer~~ or Developer fail to undertake and complete its obligations as described in this Agreement and to the City's specifications, then the City shall give the Owner/~~Developer~~ or Developer thirty (30) days written notice to commence and ninety (90) days to complete said required obligation at the sole expense of the Owner/~~Developer~~ or Developer. If the Owner/~~Developer~~ or Developer fails to complete the obligations within the ninety (90) day period, then the City, without further notice to the Owner/~~Developer~~ or Developer and their successors and assigns in interest, may but shall not be required to, perform such obligations at the expense of the Owner/~~Developer~~ or Developer or their successors and assigns in interest, without prejudice to any other rights or remedies the City may have under this Agreement. Further, the City is hereby authorized to immediately recover the actual and verified cost of completing the obligations required under this Agreement and any legal fees from the

Owner/~~Developer~~ or Developer in an action at law for damages, as well as record a lien against the Subject Property in that amount. The lien of such assessments shall be superior to all others, and all existing lienholders and mortgagees, by their execution of the subordination or joinder documents, agree to subordinate their liens or mortgages to the City's said liens or assessments. Notice to the Owner/~~Developer~~ or Developer and their successors and assigns in interest shall be deemed to have been given upon the mailing of notice as provided in paragraph (24) of this Agreement.

14. **Concurrency and Vested Rights.** The Owner/~~Developer~~ or Developer acknowledges and agrees that prior to the issuance of any development orders for the Property, the Owner/~~Developer~~ or Developer must have received and be in the possession of a valid unexpired certificate of capacity/concurrency management system approval consistent with the City's Land Development Code. The capacity certificate/approval verifies the availability of infrastructure and service capacity sufficient to permit the proposed development of the Subject Property without causing a reduction in the levels of service adopted in the City's Comprehensive Plan. The certificate of capacity/approval shall be effective for a term, as defined in the City's Code of Ordinances. Neither this Agreement nor the approved Master Development Plan shall create or result in a vested right or rights to develop the Subject Property, as cited in Section 86-34 of the City's Land Development Code.

15. **Environmental and Tree Preservation.** The property contains habitat that is suitable for scrub jays and gopher tortoises. As part of the Final Site Plan process, the property shall be surveyed to determine the presence and/or population density of scrub jays or gopher tortoises. If gopher tortoises are found to inhabit the development area then on-site preservation and/or removal shall be required in lieu of a 'take'. If scrub jays are noted on the property then any modification of scrub jay habitat shall be permitted consistent with the requirements of the U.S. Fish and Wildlife Service. Tree protection shall be in accordance with Chapter 98 of the City Land Development Code as it may be amended from time to time. Tree protection areas and habitat protection set aside land may coincide with buffer yards and other open space acreage. Irrigation shall comply with the requirements of the utility servicing the project and applicable permitting agencies. However, the use of potable water for irrigation shall be prohibited. The Owner or Developer shall install reuse infrastructure to support the irrigation and other non-potable water needs associated with the project. The Owner/~~Developer~~ or Developer is responsible to obtain all site related permits and approval prior to any development activity on or for the Subject Property. ~~This may involve mitigation for habitat of threatened or endangered flora and fauna or for species identified for proportion (i.e. tree preservation).~~ This Agreement does not vest or exempt the Owner/~~Developer~~ or Developer from any permitting and mitigation obligations needed to develop a Subject Property.

16. **Homeowners Association or Property Owners Association.** The charter and by-laws of any Homeowners Association ("HOA") or Property Owners Association ("POA") for the Subject Property and any deed restrictions related thereto shall be furnished to the City for

approval by the City Attorney prior to the recording thereof in the Public Records of Volusia County, Florida. Such recording, if applicable, shall take place before a Certificate of Occupancy is issued for the first development project on land covered by this Agreement. The HOA or POA shall at a minimum be responsible for maintaining the common open space, any common utility systems, such as for irrigation and site lighting, and project signage. The Owner/~~Developer~~ or Developer shall be responsible for establishing the HOA or POA and recording said information in the Public Records of Volusia County, Florida. The City is not responsible for the enforcement of any agreements or deed restrictions entered into between property owners or occupiers of the Subject Property. If maintenance for the Subject Property is not maintained following issuance of a Certificate of Occupancy, the City has Code Enforcement services.

17. **Enforcement.** Both parties may seek specific performance of this Agreement and/or bring an action for damages in a court within Volusia County, Florida, if this Agreement is breached by either party. In the event that enforcement of this Agreement by the City becomes necessary, and the City is successful in such enforcement, the Owner/~~Developer~~ or Developer shall be responsible for the payment of all of the City's costs and expenses, including attorney fees, whether or not litigation is necessary and, if necessary, both at trial and on appeal. Such costs, expenses and fees shall also be a lien upon the Subject Property superior to all others. Should this Agreement require the payment of any monies to the City, the recording of this Agreement shall constitute a lien upon the Subject Property for said monies, until said are paid, in addition to such other obligations as this Agreement may impose upon the Subject Property and the Owner/~~Developer~~ or Developer. Interest on unpaid overdue sums shall accrue at the rate of the lesser of eighteen percent (18%) compounded annually or at the maximum rate allowed by law.

18. **Utility Easements.** For any easement not established on a plat for the Subject Property, the Owner/~~Developer~~ or Developer shall provide to the City and/or County (as applicable) such easements and other legal documentation, in form mutually acceptable to the City Attorney and the Owner/~~Developer~~ or Developer, as the City/County may deem reasonably necessary or appropriate for the installation and maintenance of the utility and other services, including but not limited to, sanitary sewer, potable water, and reclaimed water services, electric, cable, gas, fire protection and telecommunications.

19. **Periodic Review.** The City reserves the right to review the Subject Property subject in relation to this Agreement periodically to determine if there has been demonstrated good faith compliance with the terms of this Agreement. If the City finds that on the basis of substantial competent evidence that there has been a failure to comply with the terms of this Agreement, the City may not issue development orders or permits until compliance with this Agreement has been established.

20. **Notices.** Where notice is herein required to be given, it shall be by certified mail

return receipt requested, hand delivery or nationally recognized courier, such as Federal Express or UPS. E-mail delivery of documents shall not replace or be in lieu of the aforementioned process. Said notice shall be sent to the following, as applicable:

OWNER/~~DEVELOPER'S~~ OR DEVELOPER'S REPRESENTATIVES:

[INSERT NAMES AND ADDRESSES]

CITY'S REPRESENTATIVES:

City Manager
City of Deltona
2345 Providence Boulevard
Deltona, Florida 32725

With copy to:

Director
Planning & Development Services
City of Deltona
2345 Providence Boulevard
Deltona, Florida 32725

Should any party identified above change, it shall be said party's obligation to notify the remaining parties of the change in a fashion as is required for notices herein. It shall be the Owner/~~Developer's~~ or Developer obligation to identify its lender(s) to all parties in a fashion as is required for notices herein.

21. **Compliance with the Law.** The failure of this Agreement to address a particular permit, condition, term, or restriction shall not relieve the Owner/~~Developer~~ or Developer of the Subject Property from the necessity of complying with the law governing said permitting requirements, conditions, terms, or restrictions.

22. **Captions.** The captions used herein are for convenience only and shall not be relied upon in construing this Agreement.

23. **Binding Effect.** This Agreement shall run with the land, shall be binding upon and inure to the benefit of the Owner/~~Developer~~ or Developer and their successors and assigns in interest, and the City and their successor and assigns in interest. This Agreement shall become effective upon its execution and recordation with the Public Records of Volusia County, Florida. This Agreement does not, and is not intended to, prevent or impede the City from exercising its

legislative authority as the same may affect the Subject Property.

24. **Subsequently Enacted State or Federal Law.** If either state or federal law is enacted after the effective date of this Agreement that is applicable to and precludes the parties' compliance with the terms of this Agreement, this Agreement and correlating zoning amendment shall be modified or revoked, as is necessary, to comply with the relevant state or federal law.

25. **Severability.** If any part of this Development Agreement is found invalid or unenforceable in any court, such invalidity or unenforceability shall not affect the other parts of this Development Agreement, if the rights and obligations of the parties contained herein are not materially prejudiced and if the intentions of the parties can be affected. To that end, this Development Agreement is declared severable.

26. **Covenant Running with the Land.** This Agreement shall run with the Subject Property and inure to and be for the benefit of the parties hereto and their respective successors and assigns and any person, firm, corporation, or entity who may become the successor in interest to the Subject Property or any portion thereof.

27. **Recordation of Agreement.** The parties hereto agree that an executed original of this Agreement shall be recorded by the City, at the Developer's expense, in the Public Records of Volusia County, Florida.

28. **Applicable Law/Venue.** This Agreement and the provisions contained herein shall be construed, controlled, and interpreted according to the laws of the State of Florida. Venue of any litigation relating to this Agreement shall be in the courts of Volusia County, Florida.

29. **Time of the Essence.** Time is hereby declared of the essence to the lawful performance of the duties and obligations contained in this Agreement. The Owner/~~Developer~~ or Developer shall execute this Agreement within ten (10) business days of City Commission adoption of Ordinance No. 26-2016; and agrees to pay the cost of recording this document in the Public Records of Volusia County, Florida. Failure to execute this Agreement within ten (10) business days of this ordinance adoption may result in the City not issuing development orders or permits until execution and recordation of this Agreement has occurred.

30. **Agreement; Amendment.** This Agreement constitutes the entire agreement between the parties, and supersedes all previous discussions, understandings and agreements, with respect to the subject matter hereof; provided, however, that it is agreed that this Agreement is supplemental to the City's Comprehensive Plan and does not in any way rescind or modify any provisions of the City's Comprehensive Plan. Amendments to and waivers of the provisions of this Agreement shall be made by the parties only in writing by formal amendment.

31. **Effective Date.** The Effective Date of this Agreement shall be the day this Agreement is recorded in the Public Records of Volusia County, Florida.

IN WITNESS WHEREOF, the Owner, the Developer and the City have executed this Agreement.

OWNER/DEVELOPER

By:

Signature of Witness # 1

Signature

Print or type name

Print or type name

As:

Signature of Witness #2

Print or type

ATTEST:

Print or type name

Signature

Print or Type Name

As:

Mailing Address: _____

STATE OF FLORIDA
COUNTY OF _____

The foregoing instrument was acknowledged before me this _____ day of _____, 201____, by _____, and _____, of _____, who is/are personally known to me or who has/have produced _____ as identification and who did not (did) take an oath.

(NOTARY SEAL)

Signature of Notary

Print or type name

DEVELOPER

By:

Signature

Print or type name

As:

Print or type

ATTEST:

Signature

Print or Type Name

As:

Mailing Address: _____

Signature of Witness # 1

Print or type name

Signature of Witness #2

Print or type name

STATE OF FLORIDA
COUNTY OF _____

The foregoing instrument was acknowledged before me this _____ day of _____, 201__, by _____, and _____, of _____, who is/are personally known to me or who has/have produced _____ as identification and who did not (did) take an oath.

Signature of Notary

(NOTARY SEAL)

Print or type name

CITY OF DELTONA:

By:

Date:

ATTEST:

Date:

Mailing Address:

City of Deltona

2345 Providence Boulevard

Deltona, Florida 32725

STATE OF FLORIDA

COUNTY OF _____

The foregoing instrument was acknowledged before me this ____ day of _____, 201__, by _____, and _____, who are personally known to me and acknowledge executing the same freely and voluntarily under authority vested in them by the City of Deltona.

Signature of Notary

(NOTARY SEAL)

Print or type name

Approved as to form and legality for use and

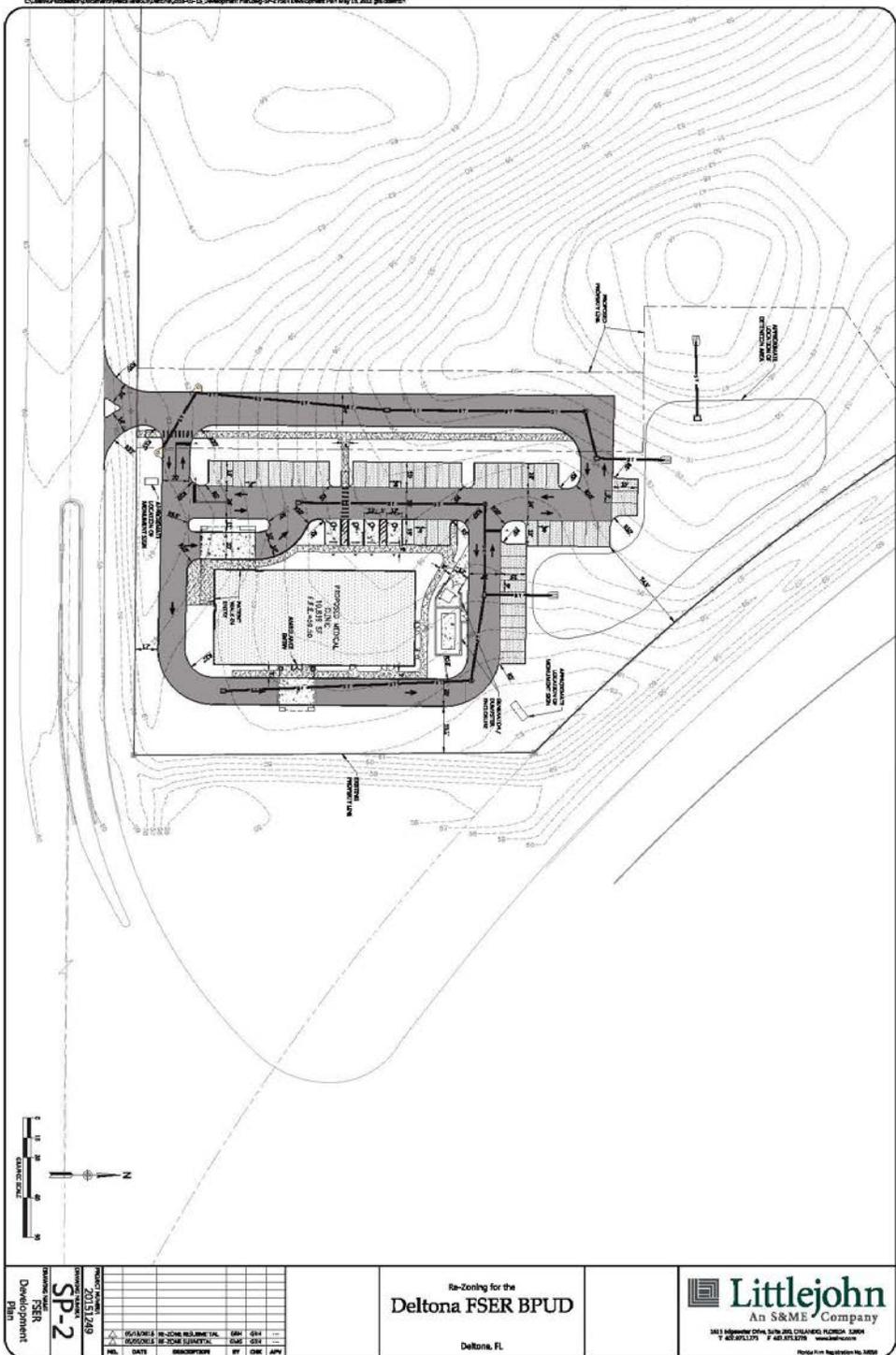
reliance by the City of Deltona, Florida

Gretchen R. H. Vose
City Attorney

EXHIBIT B
LEGAL DESCRIPTION

COMMENCE AT THE SOUTHEAST CORNER OF LOT 70, YOURLANDO FARMS AND GROVES PLAT (PLAT BOOK 10, PAGES 227 AND 228 OF THE OFFICIAL RECORDS OF VOLUSIA COUNTY, FLORIDA); THENCE ALONG THE EAST LINE OF PLATED LOT 70, NORTH 00°18'37" WEST A DISTANCE OF 10.10 FEET TO THE NORTHERLY RIGHT OF WAY LINE OF GRAVES AVENUE AND THE POINT OF BEGINNING; THENCE ALONG SAID NORTHERLY RIGHT OF WAY LINE, NORTH 89°23'10" WEST A DISTANCE OF 292.39 FEET; THENCE LEAVING SAID NORTHERLY RIGHT OF WAY LINE, NORTH 00°37'08" EAST A DISTANCE OF 381.01 FEET; THENCE NORTH 89°22'52" WEST A DISTANCE OF 50.00 FEET; THENCE NORTH 00°37'08" EAST A DISTANCE OF 150.00 FEET; THENCE NORTH 55°56'49" EAST A DISTANCE OF 114.36 FEET TO THE SOUTHWESTERLY RIGHT OF WAY LINE OF HOWLAND BLVD (S.R. 472); THENCE ALONG SAID SOUTHWESTERLY RIGHT OF WAY LINE ALONG A CURVE TO THE LEFT AND BEING CONCAVE NORTH EAST HAVING A RADIUS OF 1748.64 FEET, A CENTRAL ANGLE OF 12°32'22" AND AN ARC LENGTH OF 382.70 FEET, THE CHORD OF SAID CURVE BEARS SOUTH 38°58'46" EAST A DISTANCE OF 381.93 FEET TO THE AFOREMENTIONED EASTERLY LINE OF LOT 70; THENCE SOUTH 00° 18'37" EAST A DISTANCE OF 301.79 FEET TO THE POINT OF BEGINNING.

EXHIBIT C MASTER DEVELOPMENT



PROJECT NUMBER		20151249	
DRAWING TITLE		SP-2	
DEVELOPMENT		FSER	
ZONING		FSER	
DATE		2015	
BY		CNC	
CHK		APV	
NO.	DATE	DESCRIPTION	BY

Re-Zoning for the
Deltona FSER BPUD

Deltona, FL

Littlejohn
An S&ME Company

3815 WINTER CREEK SUITE 100 DELTONA, FLORIDA 32704
TEL: 407.271.1177 FAX: 407.271.1278 www.littlejohn.com
Florida Firm Registration No. 38828

ORDINANCE NO. 26-2016

AN ORDINANCE OF THE CITY OF DELTONA AMENDING THE OFFICIAL ZONING MAP TO REZONE APPROXIMATELY 3.24 ACRES OF LAND LOCATED IN THE SOUTHWEST CORNER OF THE INTERSECTION OF HOWLAND BLVD. AND GRAVES AVENUE FROM CITY OF DELTONA BUSINESS PLANNED UNIT DEVELOPMENT TO A NEW CITY OF DELTONA BUSINESS PLANNED UNIT DEVELOPMENT; PROVIDING FOR CONFLICTS; PROVIDING FOR SEVERABILITY; AND PROVIDING FOR AN EFFECTIVE DATE.

WHEREAS, the City of Deltona, Florida has received an application to rezone approximately 3.24 acres from City Business Planned Unit Development (BPUD) to City Business Planned Unit Development (BPUD); and

WHEREAS, the City of Deltona, Florida and its Land Planning Agency have complied with the requirements of the Municipal Home Rule Powers Act, sections 166.011 et. seq., Florida Statutes, in considering the proposed BPUD rezoning; and

WHEREAS, after said public hearing, the City Commission of the City of Deltona, Florida, has determined that the BPUD zoning is consistent with the Comprehensive Plan of the City of Deltona, Florida.

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COMMISSION OF THE CITY OF DELTONA, FLORIDA, as follows:

Section 1. Located in the City of Deltona, Florida the following property is hereby rezoned to BPUD:

COMMENCE AT THE SOUTHEAST CORNER OF LOT 70, YOURLANDO FARMS AND GROVES PLAT (PLAT BOOK 10, PAGES 227 AND 228 OF THE OFFICIAL RECORDS OF VOLUSIA COUNTY, FLORIDA; THENCE ALONG THE EAST LINE OF PLATED LOT 70, NORTH 00°18'37" WEST A DISTANCE OF 10.10 FEET TO THE NORTHERLY RIGHT OF WAY LINE OF GRAVES AVENUE AND THE POINT OF BEGINNING; THENCE ALONG SAID NORTHERLY RIGHT OF WAY LINE, NORTH 89°23'10" WEST A DISTANCE OF 292.39 FEET; THENCE LEAVING SAID NORTHERLY RIGHT OF WAY LINE, NORTH 00°37'08" EAST A DISTANCE OF 381.01 FEET; THENCE NORTH 89°22'52" WEST A DISTANCE OF 50.00 FEET; THENCE NORTH 00°37'08" EAST A DISTANCE OF 150.00

FEET; THENCE NORTH 55°56'49" EAST A DISTANCE OF 114.36 FEET TO THE SOUTHWESTERLY RIGHT OF WAY LINE OF HOWLAND BLVD (S.R. 472); THENCE ALONG SAID SOUTHWESTERLY RIGHT OF WAY LINE ALONG A CURVE TO THE LEFT AND BEING CONCAVE NORTH EAST HAVING A RADIUS OF 1748.64 FEET, A CENTRAL ANGLE OF 12°32'22" AND AN ARC LENGTH OF 382.70 FEET, THE CHORD OF SAID CURVE BEARS SOUTH 38°58'46" EAST A DISTANCE OF 381.93 FEET TO THE AFOREMENTIONED EASTERLY LINE OF LOT 70; THENCE SOUTH 00° 18'37" EAST A DISTANCE OF 301.79 FEET TO THE POINT OF BEGINNING

Section 2. This Ordinance is adopted in conformity with and pursuant to the Comprehensive Plan of the City of Deltona, the Local Government Planning and Development Act, sections 163.161 et. seq., Florida Statutes, and the Municipal Home Rule Powers Act sections 166.011 et. seq., Florida Statutes.

Section 3. Conflicts. Any and all Ordinances or parts of Ordinances in conflict herewith are hereby repealed.

Section 4. Severability. If any provision of this Ordinance or the application thereof to any person or circumstance is held invalid, such invalidity shall not affect any other provisions or applications of this Ordinance which can be given effect without the invalid provision or application.

Section 5. Effective Date. This Ordinance shall take effect immediately upon its final adoption by the City Commission.

PASSED AND ADOPTED BY THE CITY COMMISSION OF THE CITY OF DELTONA, FLORIDA THIS _____ DAY OF _____, 2016.

First Reading: _____

Advertised: _____

Second Reading: _____

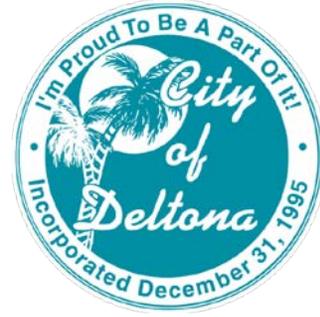
BY: _____
JOHN C. MASIARCZYK, SR., Mayor

ATTEST:

JOYCE RAFTERY, CMC, MMC City Clerk

Approved as to form and legality
for use and reliance of the City of
Deltona, Florida

GRETCHEN R. H. VOSE, ESQ, City Attorney



Staff Report

To: Planning and Zoning Board

From: Chris Bowley, AICP, Director
Planning & Development Services

Date: May 20, 2016

Re: Ordinance No. 23-2016: Amending Section 110-828, Off-street Parking and Loading Land Development Code Regulations Pertaining to Single Family Residential Lots

A. Summary of Application:

Applicant: City of Deltona (City)

Request: To amend Section 110-828, Off-street parking and loading Land Development Code (LDC) regulations pertaining to single family residential lots. The request is to accommodate additional parking on the residential lot above that provided in the original construction of the lot.

B. Background: The City of Deltona has a majority of its land subdivided into single-family residential lots. The bulk of those lots were created through the Deltona Lakes plat and others through master planned communities. As the nation becomes more mobile and the demand for automobiles and other vehicles increases, there is a need for more parking areas on a single-family residential lot. This is compounded by homeowners/tenants owning several vehicles and a lack of locations for storage.

In addition to the above, the single-family residential lot was designed, permitted, and constructed for specific basic uses. Driveways and garages are constructed to accommodate several vehicles and lots are constructed to provide for utility provision to the home and neighborhood, as well as affording positive drainage away from the foundation of the home. This includes the construction of drainage facilities and the provision of open space for water

percolation. In summary, the City's Land Development Code, the Florida Fire Prevention Code, and the Florida Building Code are designed to afford maximum development potential, while maintaining the public health, safety, and welfare with the basic use of the lot. To maintain the overarching regulations that regulate platted single-family residential lot subdivision, construction, and use, the following amendment is proposed to provide for additional off-street parking provisions on these lots.

Sec. 110-828 Off-street parking and loading.

(a) Off-street parking and loading regulations. Where required by this chapter, every use or structure shall have an adequate number of off-street parking and loading spaces for use of occupants, employees, visitors, customers, patrons or suppliers. Except as noted in this section, chapter 96, article II, of the Code of Ordinances shall apply to the design and construction of all required off-street parking and loading areas. For platted single-family residential lots, additional off-street parking may be provided, at the option of the property owner, where it is physically possible and legally permissible on the lot without creating a nuisance, provides for safe ingress and egress, does not alter the design, permitting, construction, and drainage patterns of the lot, is not within easements, is not located on a utility system, and adheres to the City's Land Development Code, the Florida Fire Prevention Code, and the Florida Building Code.

CONCLUSION/STAFF RECOMMENDATION:

The proposed amendment to Section 110-828(a) of the City's Land Development Code (LDC) allows for the maximum flexibility and use of a platted single-family residential lot, while also providing for additional off-street parking above and beyond the original design, permitting, and construction of the lot. Based on the above, staff recommends approval of Ordinance No. 23-2016.

Sec. 110-828. - Off-street parking and loading. (Regulations)

- (a) Off-street parking and loading regulations. Where required by this chapter, every use or structure shall have an adequate number of off-street parking and loading spaces for the use of occupants, employees, visitors, customers, patrons or suppliers. Except as noted in this section, chapter 96, article II, Code of Ordinances shall apply to the design and construction of all required off-street parking and loading areas.
- (b) Surfacing, drainage, lighting, and access.
 - (1) For single-family and two-family (duplex) residential dwellings, for model homes and model home parking lots located in residential zoning categories, and for city-owned facilities and Volusia County, or State of Florida, park and recreation facilities, off-street parking areas and driveways that connect to paved streets or roads shall be surfaced with either concrete, brick, or asphalt, and maintained in a good condition for a minimum distance of 35 feet, or the distance to the house from the front or street side lot line, whichever is less. Additional driveways, driveways that connect to unpaved streets or roads, and parking spaces on these sites may be surfaced with crushed rock, shell, or stone, and maintained in a smooth well-graded condition.

Melissa suggestion add mulch, brick pavers

Material used at grades exceeding five percent (20'h:1'v) must be attached to the ground, and may include paving brick or stone. Borders shall include provisions to ensure stormwater runoff is allowed to flow into the yard area adjacent to the driveway. Stormwater shall not be directed down the driveway into the public or private right-of-way or access easement except to the minimum extent necessary to effect a connection to the driveway apron at the property line, or onto adjacent property. The design storm event shall be as specified in the Deltona Land Development Code as it may be amended from time to time.

Joe suggestion Based on the minimum/maximum spaces – would a standard driveway of a single family home that can accommodate 4 vehicles be in violation of the maximum allowable off-street parking spaces (see Table 110-9).

Joe suggestion b)(1) – add sentence as follows: Any additional parking adjacent to a paved or concrete driveway may be surfaced with mulch, rock, shell or stone and maintained in a smooth well-graded condition.

- (2) Any required off-street parking and loading areas, including overflow areas, for land uses other than single-family or two-family (duplex) dwellings shall be surfaced with brick, asphalt, bituminous, or concrete material and maintained in a smooth condition.

Melissa suggestion add mulch, rock – & Question what about Church facilities that have dirt/pine needle and wood markers for parking spaces?

All areas shall be designed for the safety and convenient access of pedestrians and vehicles. An illumination plan prepared by a licensed professional engineer with expertise in the field of illumination, including the latest illumination technology available, shall be submitted for each parking facility that is proposed to be illuminated. Lights, used to illuminate any off-street parking facility, shall be designed and installed to prevent a related hazard or nuisance to vehicular or air traffic and to prevent glare, annoyance or discomfort by directing light away

from adjacent residential properties and adjoining streets. In no case shall illumination from a parking facility (including illuminated canopies) increase the level of illumination at the edge of pavement on adjacent streets or at the property lines of adjoining properties by more than one foot candle. The maximum height of the light fixture, including pole and lamp head, shall not exceed 35 feet and be erected such that any series of light poles and lamp heads are equal in height, as measured from ground level.

- (3) To promote the safety of vehicular traffic and pedestrians and to minimize traffic congestion and conflict, access to any project or development, including single-family homes and duplexes, shall comply with the requirements of the Land Development Code, chapter 96, article II, as it may be amended from time to time, and shall require a hard surface driveway apron meeting city engineering standards connecting the driveway to a public street.

(c) Location.

- (1) The minimum number of parking spaces required in section 110-828(f) for all single-family and two-family dwellings shall be located on the same lot as the main building. If additional parking spaces are required for any single-family or two-family dwelling, the additional parking spaces may be located either on the same lot as the main building, or on an adjacent vacant lot of an expanded residential building site. If the required off-street parking spaces for all other uses cannot reasonably be provided on the same lot on which the principal building or use is located, such required off-street parking spaces may be located on another lot, owned or leased by the owner of the lot on which the principal structure or use is located, provided that such spaces are located within 200 feet of the premises to be served, and, are located only in one or more of the following classifications: RM-1, RM-2, OR, C-1, C-2, C-3, I, PUD or PB. Such spaces may be located in any single-family residential zoning district only as a conditional use. Heavy equipment and vehicles requiring a commercial drivers' license of any class shall not be parked or stored on an off-premises parking lot permitted by this paragraph within any residential zoning district, or within the professional business zoning classification.

- (2) No parking space or portion of any parking facility shall be located or built within any platted easement unless an authorized use permit is issued by the City of Deltona.

(d) Plan requirement. An off-street parking or loading space plan shall be submitted as follows:

- (1) For single-family and duplex uses off-street parking plans shall be shown on the plot plans submitted with an application for a building or zoning permit. The plot plan shall accurately illustrate the number and location of parking spaces and driveways. The addition of parking spaces to an existing single-family or duplex residential building site shall require an application for a zoning permit, which shall include a plot plan and sealed survey of the expanded residential building site. All required landscaping and screening, areas proposed to be cleared, trees proposed to be removed, existing and proposed vehicle access ways, parking areas, and structures shall be shown on the plot plan. In addition, building permit applications shall be submitted for any proposed construction that is regulated by the city's adopted building code.

- (2) For all other uses, an off-street parking and loading space plan meeting the requirements of chapter 96, article II, Code of Ordinances shall be submitted and approved during the site plan review process of the Land Development Code.

(e) Design requirements for off-street parking areas. Off-street parking areas shall be designed and located to meet the following requirements:

- (1) For single-family and duplex uses, except as otherwise provided in this article for expanded residential building sites, each off-street parking space shall be located on the premises which it serves; have minimum dimensions of nine feet in width by 19 feet in depth; not be located in any front yard except on a driveway but may be located within any garage or carport on the premises; and/or, may be located within any side or rear yard but not closer than five feet to any side or rear lot line, but not in any platted easements unless an authorized use permit is issued by the City of Deltona. ~~Each such space must be accessible from a driveway connected to the street providing primary access to the premises.~~

Melissa suggest to omit due to the unique property designs of homes in Deltona

The design requirements for parking on the vacant lot of an expanded residential building site are set forth in paragraph (2) of this section.

- (2) When additional parking is installed on the vacant lot of an expanded residential building site, the parking area shall be designed and built in accordance with the requirements of this paragraph, as follows:
 - a. Surfacing, drainage and access. Surfacing, drainage and access for any parking area on the adjacent vacant lot of an expanded residential building site shall meet the requirements of subsection 110-828(b).
 - b. Driveway spacing. Driveway spacing shall meet the minimum standards of the Deltona Land Development Code, Ordinance 96-25, as it may be amended from time to time. No driveway connection to a street may be made to the vacant portion of a residential building site for the purpose of providing additional parking. Access shall be provided across the adjacent lot on which a one- or two-family dwelling exists. The driveway or access way serving the parking facility on the vacant lot of an expanded residential building site shall be built using one of the types of surfacing required for parking areas in subsection 110-810(b), as it may be amended from time to time.
 - c. Driveways, access ways, and parking areas. These facilities shall not be built in a manner that impairs any easement.
 - d. Setbacks. Any additional parking area on the vacant portion of an expanded residential building site shall be no closer to the front or rear lot lines than 30 feet, and no closer to the exterior side lot line of the vacant lot than 20 feet. The exterior side lot line shall be the lot line directly opposite the common lot line that lies between the two lots comprising an expanded residential building site.
 - e. Natural vegetation. Existing natural vegetation shall not be cleared from the area within the minimum setbacks required in paragraph "d." of this subsection, except to remove hazards or nuisance vegetation limited to exotic species, vines, poisonous plants, and dead or diseased plants, and in accordance with the tree protection requirements of chapter 98, article II of the Deltona Land Development Code as it may be amended from time to time.
 - f. Lot clearing. Lot clearing shall be limited to an area beyond the setbacks specified in paragraph "e." of this subsection that shall not exceed 55 feet in depth measured from the common lot line of the expanded residential building site toward the opposite side lot line, and 50 feet in width. However, clearing to the maximum allowable width and depth shall not be permitted in every instance, but shall be limited to those instances where

that is the minimum amount of clearing required to accommodate the types of vehicles being parked on the vacant lot of the expanded residential building site. In each case, clearing shall be limited in width to the minimum needed to accommodate one parking space of nine feet in width for each vehicle proposed to be parked plus a maximum of ten feet on each side to accommodate any visual screening required by paragraph "g." of this section. In each case clearing shall be limited in depth to a depth that equals the length of the vehicle proposed to be parked plus a maximum of an additional ten feet to accommodate any visual screening required by paragraph "g." of this section.

- g. Visual screening. If any portion of the parking area provided on the vacant portion of an expanded residential building site is visible from any adjacent street or lot, except the portion facing the common lot line of the site, that portion of the parking area must be screened from view. Minimum screening required shall be either a 100 percent opaque hedge, fence or wall at least four feet, but not more than six feet in height, and a row of understory trees high enough that their crowns obscure the parked vehicles from view. Chain link fences with cover materials or inserts shall not be permitted to meet this screening requirement. The required understory trees shall be planted so that their crown spreads at maturity shall completely cover the area within which any parked vehicle is visible. The required shrubs and understory trees shall reach maturity and achieve the required minimum screening within two years from the date of planting. All required landscaping materials shall be of the species specified in the approved plant species list in section 110-808 of the Land Development Code of the City of Deltona, as it may be amended from time to time. Except that deciduous species that drop their all or most of their leaves at any time of year are prohibited to be used as screening materials meeting the requirements of this section.

(3) For all other uses, off-site parking and loading areas shall be designed and located according to the requirements of this ordinance and the applicable articles of the Land Development Code, Ordinance No. 96-25, as it may be amended from time to time.

- (f) Minimum off-street parking spaces. Minimum off-street parking spaces shall be provided with adequate means for vehicle ingress and egress from a public street or alley by an automobile of standard size, in accordance with the following table. Fractional spaces shall be rounded to the closest whole number. In stadiums, houses of worship, sports arenas, or other places of assembly where occupants sit on seats without dividing arms, each 18 linear inches of such seat shall be counted as one seat.

The minimum and maximum number of parking spaces required for any use not specifically mentioned, shall be determined by the zoning enforcement official or his or her designee based upon data from the Institute of Transportation Engineers Parking Generation Manual, from publications and data from the American Planning Association or the Urban Land Institute, from studies using ITE recommended methodology and other professionally acceptable sources. Information that other land uses, which are the same as, or similar to, the land use for which a parking determination is sought, have been provided a given number of parking spaces in other jurisdictions shall not be controlling in determining parking requirements, unless such requirements in other jurisdictions are supported by publications, data and information available, or presented in writing, to the zoning enforcement official.

Table 110-9: Minimum Off-Street Parking Spaces

Land Use	Number of Parking Spaces
Parks:	
Open "free play area"	8 spaces per acre.
Equipped playground	10 spaces per site.
Multipurpose court	5 spaces per court.
Picnic area	1 space per table.
Baseball/softball	38 spaces per field.
Handball/racquetball court	2 spaces per court.
Tennis court	2 spaces per court.
Soccer/football	34 spaces per field.
Shuffleboard court	2 spaces per court.
Basketball court	5 spaces per court.
Jogging/fitness trail	2 spaces per trail.
Multipurpose field	8 spaces per acre.
Primitive camping	1 space per site.
Fishing pier	4 spaces per 50 lineal feet.
Boat ramp	36 spaces per boat lane.
Volleyball	6 spaces per court.
Concession building	1 space per concessionaire or employee.
Community center	1 space per 200 square feet GFA.*

Community pool—50 meter (164' × 75')	91 spaces.
Neighborhood pool—25 yards (75' × 45')	25 spaces.
Transportation centers	1 space for each 4 estimated average daily passengers To be addressed in the future—Multi-modal.
Recycling collection center	1 space per employee.
Solid waste transfer station	1 space per employee.
Banks	4 spaces/1,000 square feet of GFA with 5 reservoir spaces per drive thru window and drive thru ATM.
One- and two-family homes	1 BR and more: 2 spaces/d.u., in addition to garage parking, if any.
Multi-family	<p>Studio and 1 BR: 1.5 spaces/d.u. + 1 space per 10 d.u. for guest parking.</p> <p>2 BR: 2 BR or more: 2.0 spaces/d.u. + 1 space per 10 d.u. for guest parking.</p> <p>No recreational vehicles, boats, or trailers are permitted except within an additional visually screened and secured parking area that may be provided specifically for recreational vehicle, boat, and/or trailer storage.</p> <p>Board discussed prohibiting parking on side lots between houses</p> <p>Melissa suggestion to omit, and to allow for off-street parking area for uninhabited recreational, vehicle not to exceed the 19ft requirement in (e)(1)</p>
Hardware store	2.86 spaces /1,000 square feet GFA.
Home improvement superstore	3.5 spaces/1,000 square feet GFA** applicant may increase to 5 spaces/1,000 GFA provided additional spaces may be classified as alternative surface spaces when located greater than 300 feet from the front door.*Garden Center area shall be included.

Hotels, motels	1 space/rental unit 1 space/guest room, plus an additional 10% for employees, plus additional parking required for any other land uses on the site, such as restaurants or bars.
Hospitals	1 space per bed.
Colleges, community colleges, or other places of higher learning	1.25 spaces per student and a minimum of 1 space per 250 square feet GFA of office area up to 1,000 office spaces, and 1 space per 500 GFA of office area for offices over a campus total of 250,000 square feet of GFA office area and 1 space per 200 square feet GFA devoted to classrooms, plus minimum parking required for other areas of assembly, sports arenas, and stadiums.
Nursing and convalescent facilities	1 space/2 patient beds, based on the maximum designed capacity of the facility.
Live theater or auditorium	1 space/3 persons permitted at fire code maximum occupancy plus 10% for employee parking.
Movie theater	1 Screen - 1 space/3 seats permitted at maximum fire code occupancy, plus 10% for employee parking. 2 Screens - 1 space/4 seats permitted at maximum fire code occupancy, plus 10% for employee parking. >2 Screens - 1 space/5 seats permitted at maximum fire code occupancy, plus 10% for employee parking.
Church	1 space/3 seats in main assembly area, or 33.3 spaces/1,000 square feet GFA in main assembly area if no fixed seating is provided. Seating shall be based on maximum fire code occupancy. Plus parking required for other uses on the site that operate during hours when the main assembly area may be in use.
Mortuary or funeral home	1 space/3 seats in main assembly area, or 33.3 spaces/1,000 square feet GFA in main assembly area if no fixed seating is provided. Seating shall be based on maximum fire code occupancy.
Other places of assembly	1 space/3 seats in main assembly area, or 33.3 spaces/1,000 square feet GFA in main assembly area if no fixed seating is provided. Seating shall be based on maximum fire code occupancy.

Restaurants	<p>Type A: Indoor service, low turnover: 12 spaces/1,000 square feet GFA indoor service; High turnover: 14 spaces per 1,000 square feet GFA, plus 6 reservoir spaces/service lane, with a minimum of 3 spaces behind the order station or menu.</p> <p>Type B: Fast food: 6 reservoir spaces/ service lane with a minimum of 3 spaces behind the order station or menu, plus 10 spaces/1,000 square feet GFA.</p> <p>Restaurant without customer seating 1 space/100 square feet GFA.</p>
Bars and night clubs	1 space/100 square feet GFA.
Office buildings less than 25,000 square feet	3.25 spaces/1,000 square feet GFA.
Office buildings between 25,000 sq. ft. and 500,000 square feet	3.0 spaces/1,000 square feet GFA.
Office buildings over 500,000 square feet	Determined by parking demand study using professionally acceptable parking demand methodology approved by the zoning enforcement official, with results to be reviewed and approved by the city, but not less than 3 spaces/1,000 square feet GFA.
Medical offices, clinics, and laboratories	<p>Min: 5 spaces/1,000 square feet GFA.</p> <p>Max: 6 spaces/1,000 square feet GFA.</p>
Retail sales and services, general	Less than 150,000: 4.44 space/1,000 square feet GFA 150,000 square feet or greater: 4.0 space/1,000 square feet GFA. Garden center area shall be included.
Shopping centers	4.0 spaces/1,000 square feet GFA. Garden center area shall be included.
Library	5.0 spaces/1,000 square feet GFA.
Automated service station retail sales (Types A and B)	1 space/gas pump, plus 3 spaces/service bay (not including service bays as parking areas)
Automobile service station with	1 space/gas pump, plus 5 spaces/1,000 square feet GFA.

retail sales (Type C)	
Government office building	4 spaces/1,000 square feet GFA plus required seating auditorium when an assembly area is included.
Manufacturing	1.54 spaces/1,000 square feet GFA for manufacturing, plus and Industrial Required parking for other uses on the site.
Commercial and industrial dead storage	4 spaces/1,000 square feet.
Contractors storage yards	1 space/1,000 square feet.
General warehouses	1.54 spaces/1,000 square feet GFA, plus 3.5 spaces/1,000 square feet of office or retail area.
Self-service and mini-warehouses	1 space/10 cubicles or units, plus continuous Loading spaces clear of through traffic access, plus 3.5 spaces/1,000 square feet of office area.
Bowling alleys	4 spaces/alley, plus required parking for other uses on the site, plus 10% for employees.
Private clubs and lodges	1 space/3 persons permitted at the maximum fire code capacity of the main assembly area.
Day care center	1 space/state required staff, plus 1 space/5 children at maximum permitted capacity, plus 1 passenger loading space/ea. 10 children under care, minimum 4 spaces. In lieu of the above requirement, a two lane loading and access area may be provided in accordance with the design standards for such loading and access areas in the Deltona Zoning Ordinance, as it may be amended from time to time. When such loading and access area is provided, the minimum parking requirement shall be reduced to 1 space/10 children at maximum licensed occupancy, plus 1 space/state required staff person.
Group home	1 space/5 resident clients, plus 1 space/state Required employee on the largest shift.
Bed & breakfast	1 space/guest room, plus 10% for employee/guest parking, plus 2 spaces/permanent resident dwelling unit.

Ball park or stadium, not including Little League and similar children's recreational programs	1 space/ea. 3 seats or 1/300 square feet GFA, whichever is greater.
Recovery homes	1 space/2 patient beds, plus 10% for staff. If visitation is allowed, add another 10% for visitors.
Pool halls and billiard parlors	2 spaces/pool and billiard table, plus required parking for all other uses on the site, including restaurants or bars.
Golf or country club	6 spaces/golf hole, plus required parking for any other uses on the site.
Swim club	1 space/250 square feet member use area in principal building, plus 1 space/50 square feet of pool and deck area, 1 space per three (3) seats in any spectator area, and required parking for any other uses on the site.
Rooming or boarding houses and dormitories	1.5 spaces/rented room or unit, or 1 space/400 square feet boarding houses GFA when dormitory style facilities are provided.
Fraternities or sororities or student cooperatives	1 space/2 occupants based on fire rated capacity of the building.
Mobile home parks	2 spaces/d.u. plus any additional spaces required to service accessory buildings or structures, plus required parking for all other uses on the site.
Car wash	Full service: 1 space per employee on maximum shift plus sufficient area for stacking spaces. Self service: 1 stacking space per washing bay.
Veterinary clinics	4.44 space/1,000 square feet GFA.
Telemarketers	10 space/1,000 square feet GFA.
Furniture stores	2 spaces/1,000 square feet GFA.
Health club and spas	5.71 space/1,000 square feet GFA.
All land uses, maximum	Maximum permitted number of parking spaces shall not exceed 1.15

permitted parking	times the minimum parking required by this section, unless otherwise stated herein, or unless a waiver of the maximum parking limitation is obtained from the zoning enforcement official pursuant to this section.
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- (g) Minimum requirements for off-street handicapped parking. Except for standard and manufactured single-family dwellings, and two-family standard or manufactured dwellings, where off-street parking spaces are required by this chapter, the number to be reserved for the handicapped shall be determined from the following table.

Table 110-10: Minimum Requirements for Off-Street Handicapped Parking

Total No. of Off-Street Parking Spaces	No. of Spaces Required to be Reserved for Handicapped
Up to 25	1
26 to 50	2
51 to 75	3
76 to 100	4
101 to 150	5
151 to 200	6
201 to 300	7
301 to 400	8
401 to 500	9
501 to 1,000	2 percent of total
Over 1,000	20 plus 1 for each 100 over 1,000

- (h) Off-street loading and unloading regulations. At the time of construction of one of the following categories of buildings, or at the time of structural alteration for an increase in size or capacity, there shall be provided minimum off-street loading or unloading spaces with adequate means of ingress and egress from a public street or alley, without interfering with the public use of streets, or off-street parking spaces. Off-street parking spaces may not be used to meet off-street loading requirements.

The dimensions, design, and location of all off-street loading spaces shall meet the requirements of article IV of the Land Development Code.

The minimum numbers of off-street loading spaces shall be determined from the following table:

Table 110-11: Off-Street Loading Spaces

Use Category	Floor Area in Square Feet	Loading Space Required
Retail sales and service, restaurants or similar uses	3,000—10,000	1
	10,001—20,000	2
	Each additional 20,000 square feet or fraction	1
Offices, hotels, hospitals, nursing homes, assisted living facility, multi-family dwellings or similar uses	30,000—100,000	1
	Each additional 100,000 square feet or fraction	1
Arenas, auditoriums, stadiums, convention centers, exhibition halls, museums or similar uses	10,000—50,000	1
	50,001—100,000	2
	Over 100,000	4
Any industrial use and any wholesale, retail and commercial storage facility	15,000—40,000	1
	40,001—100,000	2

	100,000—160,000	3
	Each additional 80,000 square feet or fractions	1

- (i) Bicycle parking regulations. Each of the following uses shall be required to provide parking spaces for bicycles: parks and recreation areas; convenience stores; restaurants (Types A and B); game rooms; pharmacies; shopping centers (regional, community, and neighborhood); and any employment facility (i.e., office, industrial) with at least 50 employees.

The minimum number of bicycle spaces to be provided shall be determined from the following table:

Table 110-12: Minimum Number of Bicycle Spaces

Required No. of Automobile Parking Spaces	Minimum Number of Required Bicycle Parking Spaces
1—40	2
41—60	3
61—80	4
81—100	5
Over 100	6 plus 1 for each 20 automobile parking spaces over 100, provided that the maximum number of required bicycle spaces shall not exceed 20.

All bicycle parking shall be located so as to not conflict with automobile or pedestrian traffic flow.

- (j) Mass transit parking requirements. Community and regional shopping centers shall be designed to accommodate buses for convenient and safe boarding and unloading of passengers as well as maintaining a safe traffic pattern. Shopping centers of greater than 100,000 square feet of gross leasable floor area shall provide a passenger shelter or covered benches to accommodate the mass transit system riders. Bus stops, shelters, and benches shall be designed so as to avoid interference with automobile and pedestrian traffic from mass transit operations and facilities.

(Ord. No. 35-00, §§ 1, 2, 11-7-2000; Ord. No. 26-01, § 1, 9-17-2001; Ord. No. 28-01, § 2, 10-15-2001;
Ord. No. 06-2003, § 3, 11-3-03; Ord. No. 27-2006, § 1, 11-6-2006; Ord. No. 06-2007, §§ 4, 5, 4-16-2007;
Ord. No. 19-2011, § 1(Exh. A), 11-7-2011)

ORDINANCE NO. 23-2016

AN ORDINANCE OF THE CITY OF DELTONA, FLORIDA, AMENDING SECTION 110-828, “OFF-STREET PARKING AND LOADING,” OF ARTICLE VIII, “SUPPLEMENTARY REGULATIONS”, OF CHAPTER 110 “ZONING”, OF THE LAND DEVELOPMENT CODE OF THE CITY OF DELTONA; PROVIDING FOR AMENDMENTS TO ALLOW ADDITIONAL PARKING ON SINGLE-FAMILY RESIDENTIAL LOTS; PROVIDING FOR CONFLICTS, CODIFICATION, SEVERABILITY, AND AN EFFECTIVE DATE.

WHEREAS, the City of Deltona adopted Article VIII, Supplementary Regulations, and Chapter 110, Zoning, of the City’s Land Development Code; and

WHEREAS, Section 110-828, of Chapter 110 provides regulations for off-street parking, inclusive of regulations for off-street parking on single-family residential lots, as designed, permitted, and constructed within the City; and

WHEREAS, the City of Deltona has the majority of its platted lands in single-family residential lots; and

WHEREAS, the City Commission of the City of Deltona, Florida, finds that providing additional off-street parking on single-family residential lots affords the ability to park additional vehicles in the form of several automobiles, boats, recreational vehicles, trailers, among others, and that the provision of additional parking on a single-family residential lot as limited shall not create a nuisance; and protects the public health, safety and welfare of the community.

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COMMISSION OF THE CITY OF DELTONA, FLORIDA, as follows:

Section 1. Section 110-828, Off-street parking and loading, of the City’s Code of Ordinances is hereby amended to read as follows:

Sec. 110-828 Off-street parking and loading.

(a) *Off-street parking and loading regulations.* Where required by this chapter, every use or structure shall have an adequate number of off-street parking and loading spaces for use of occupants, employees, visitors, customers, patrons or suppliers. Except as noted in this section, chapter 96, article II, of the Code of Ordinances shall apply to the design and construction of all required off-street parking and loading areas. For platted single-family residential lots, additional off-street parking may be provided, at the option of the property owner, where it is physically possible and legally permissible on the lot without creating a nuisance, provides for safe ingress and egress, does not alter the design, permitting, construction, and drainage patterns of the lot, is not within easements, is not located on a utility system, and adheres to the City's Land Development Code, the Florida Fire Prevention Code, and the Florida Building Code.

Section 2. Conflicts. All Ordinances or parts of Ordinances, insofar as they are inconsistent or in conflict with the provisions of this Ordinance, are hereby repealed to the extent of any conflict.

Section 3. Codification. The provisions of this Ordinance shall be codified as and become and be made a part of the Code of Ordinances of the City of Deltona. The sections of this Ordinance may be renumbered or re-lettered to accomplish such intention.

Section 4. Severability. If any provision of this ordinance or the application thereof to any person or circumstance is held invalid, such invalidity shall not affect any other provision or application of this ordinance which can be given effect without the invalid provision or application.

Section 5. Effective Date. This Ordinance shall take effect immediately upon its final adoption by the City Commission.

**PASSED AND ADOPTED BY THE CITY COMMISSION OF THE CITY OF
DELTONA, FLORIDA THIS _____ DAY OF _____, 2016.**

First Reading: _____

Advertised: _____

Second Reading: _____

BY: _____
JOHN C. MASIARCZYK, SR., Mayor

ATTEST:

JOYCE RAFTERY, CMC, MMC, City Clerk

Approved as to form and legality
for use and reliance of the City of
Deltona, Florida

GRETCHEN R. H. VOSE, City Attorney